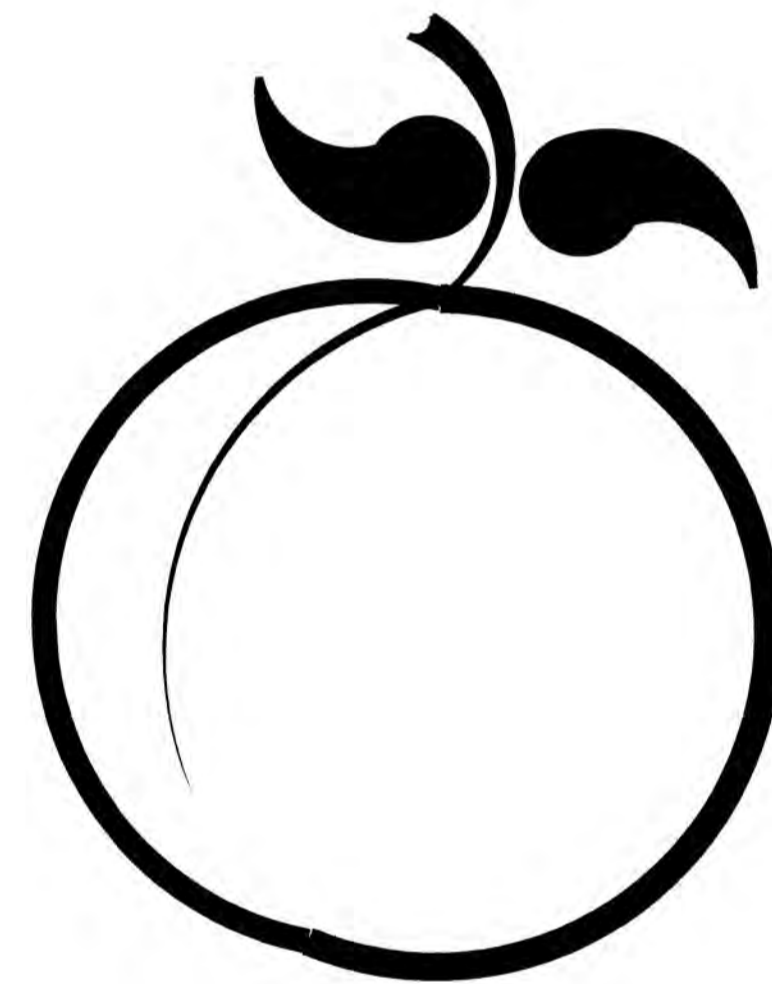


DRAWINGS FOR

# COMMERCE ROAD & FRONTAGE ROAD PAVING



## LEXINGTON, NEBRASKA

**M&A PROJECT NO. 127-A1-104**



MILLER & ASSOCIATES Consulting Engineers P.C.  
1111 Central Avenue - Kearney, NE 68847



SET NO. \_\_\_\_\_  
DRAWING NO. 35318

UTILITIES

GENERAL DIGGERS HOT LINE  
8-1-1



COMMERCE ROAD & FRONTAGE ROAD PAVING  
**LOCATION MAP & INDEX**  
LEXINGTON, NEBRASKA



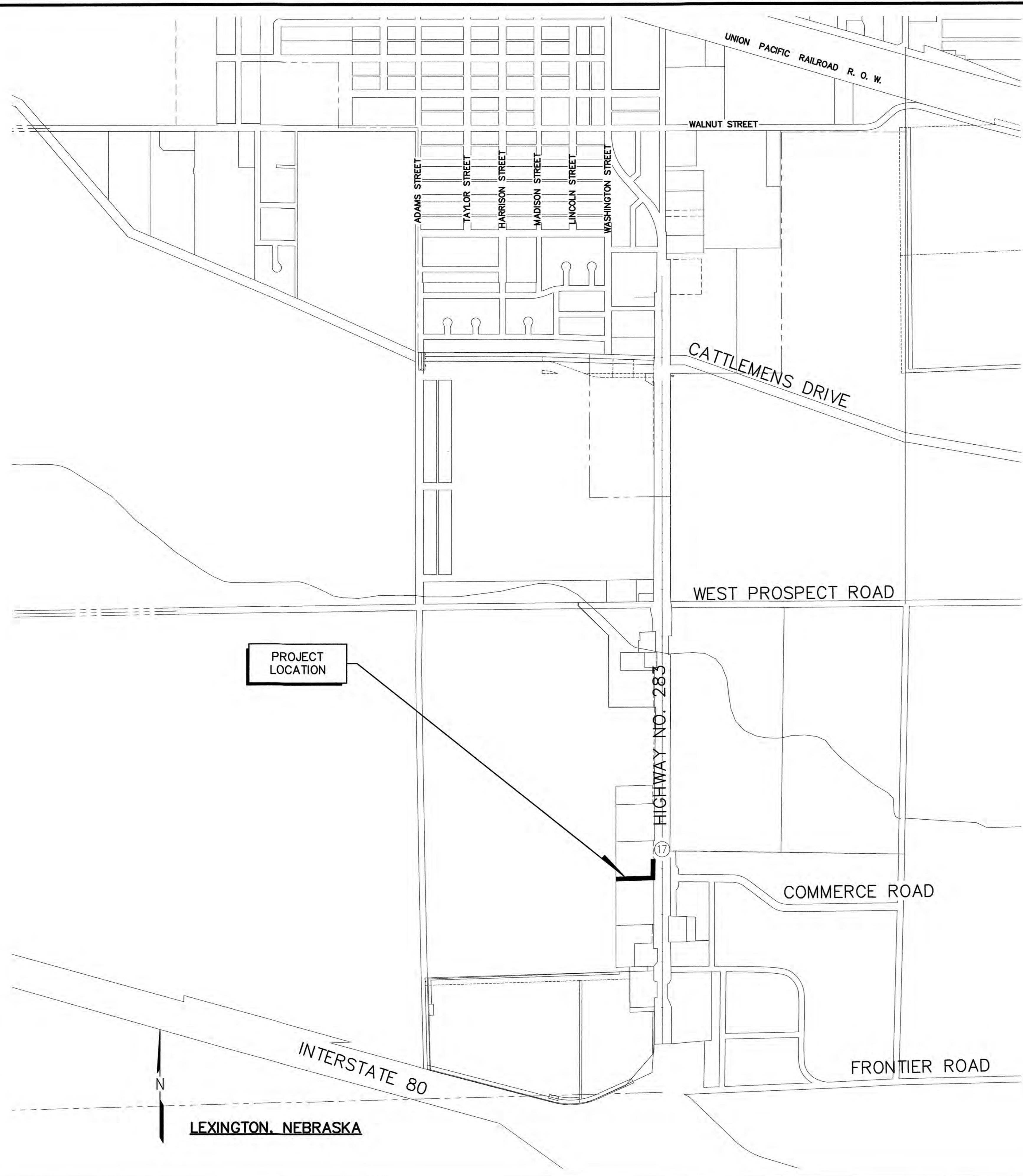
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DATE: AUGUST, 2018	M&A DWG NO. 35317
DRAWN BY: ILS	APRVD BY: TRJ
SHEET	2

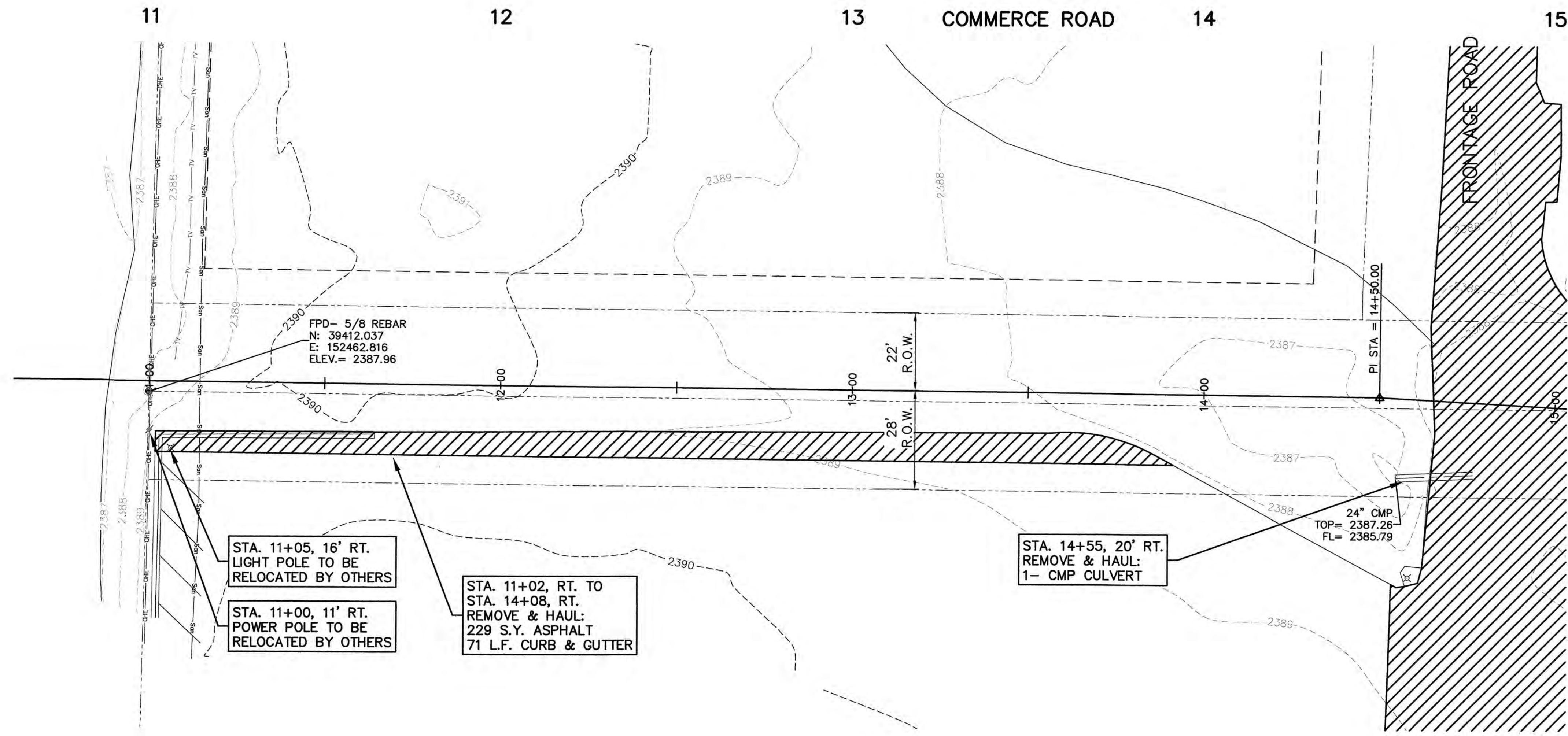
DRAWING INDEX	
SHT. NO.	DESCRIPTION
1	TITLE SHEET
2	LOCATION MAP & INDEX
3	REMOVALS - COMMERCE ROAD
4	REMOVALS - FRONTAGE ROAD
5	PLAN & PROFILE - PAVING
6	PLAN & PROFILE - PAVING
7	EROSION CONTROL PLAN
8	STORM SEWER & PAVING DETAILS
9	STORM INLET DETAILS
10	PAVING DETAILS
11	HANDICAP RAMP DETAILS
12	EROSION CONTROL DETAILS
X1 - X4	COMMERCE ROADWAY CROSS SECTIONS
X5 - X7	FRONTAGE ROADWAY CROSS SECTIONS

SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
	HEDGES, BUSHES, SHRUBS OR WOODS		BASE LINE
	DECIDUOUS TREE		BASE LINE WITH POINT OF INTERSECTION
	CONIFEROUS TREE		PERMANENT EASEMENT
	TREE STUMP		TEMPORARY EASEMENT
	CONCRETE, ASPHALT STREETS, ROADS OR DRIVES		GAS METER OR REGULATOR
	GRAVEL STREETS, ROADS OR DRIVES		WATER METER
	CONCRETE WALKS OR SLABS		PROPOSED WATER VALVE
	CURB AND GUTTER		EXISTING WATER VALVE
	CATCH BASIN		FIRE HYDRANT
	POWER POLE		CURBSTOP
	LIGHT POLE		YARD HYDRANT
	GUY WIRE		PROPOSED-WATER SERVICE
	STREET SIGN		EXISTING-WATER SERVICE
	TELEPHONE RISER		EXISTING-WATER MAIN
	FLAG POLE		EXISTING STORM SEWER
	EXISTING STRUCTURE		EXISTING SANITARY SEWER
	MAILBOX		MANHOLE
	FENCE-LIGHT WOOD OR WIRE		CLEANOUT
	CULVERT		UNDERGROUND ELECTRICAL LINE
	RAILROAD TRACKS		UNDERGROUND TELEPHONE LINE
	PROPERTY PIN		UNDERGROUND GAS LINE
	CENTERLINE		DRAINAGE COURSE OR FLOW LINE
	PROPERTY OR ROW LINE		EXISTING GROUND CONTOUR
	RECOVERY WELL/MONITORING WELL		NEW GROUND CONTOUR

- NOTES:**
- CONTRACTOR IS TO COORDINATE ALL STREET CLOSINGS WITH THE CITY OF LEXINGTON. NOTIFY FIRE/RESCUE DEPARTMENT, AND ALL ADJOINING PROPERTY OWNERS 72 HOURS PRIOR TO CLOSING STREETS OR DRIVES, OR AS NOTED IN THE SPECIFICATIONS.
  - THE LOCATION OF ALL AERIAL AND UNDERGROUND UTILITY FACILITIES MAY NOT BE INDICATED IN THESE DRAWINGS. UNDERGROUND UTILITIES, WHETHER INDICATED OR NOT, WILL BE LOCATED AND FLAGGED BY THE UTILITIES AT THE REQUEST OF THE CONTRACTOR.
  - NO EXCAVATION WILL BE PERMITTED IN THE AREA OF UNDERGROUND UTILITY FACILITIES UNTIL ALL SUCH FACILITIES HAVE BEEN LOCATED AND IDENTIFIED TO THE SATISFACTION OF ALL PARTIES AND THEN ONLY WITH EXTREME CARE TO AVOID ANY POSSIBILITY OF DAMAGE TO THE UTILITY FACILITY.
  - ALL SIGNS, MAILBOXES, CULVERTS, GUYWIRES, FENCES, AND PROPERTY PINS ENCOUNTERED DURING THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
  - THE QUANTITIES OF EACH ITEM LISTED ON THE BID SCHEDULE MAY VARY FROM THE TOTAL QUANTITIES. THE CITY MAY ADD OR DELETE FROM THE PROJECT, PORTIONS OF IMPROVEMENTS.
  - SAW CUTTING OF PAVEMENT IS REQUIRED FOR REMOVAL AND IS INCIDENTAL TO THE PROJECT. ADDITIONAL SAW CUTTING BEYOND THAT WHICH IS INDICATED ON THE DRAWINGS MAY BE REQUIRED.
  - TOPSOIL SHALL BE REMOVED, STOCKPILED AND REPLACED FOR ALL EXCAVATION AREAS.
  - CONTRACTOR SHALL GUARD AND PROTECT ALL TREES, SHRUBS, FENCES, AND OTHER ITEMS NOT SPECIFICALLY MARKED FOR REMOVAL.
  - THE CONTRACTOR MAY CLOSE THE ROAD TO ALL BUT LOCAL TRAFFIC SUBJECT TO THE CONDITIONS PRESCRIBED IN THE STANDARD SPECIFICATIONS.
  - THE CONTRACTOR SHALL FIND A SUITABLE LOCATION BEYOND THE LIMITS OF THE RIGHT-OF-WAY TO DISPOSE OF EXCESS MATERIAL.



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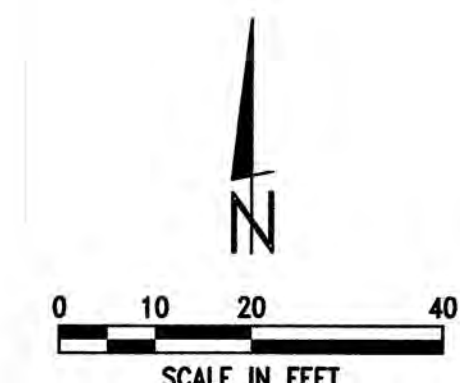


STA. 11+05, 16' RT.  
 LIGHT POLE TO BE  
 RELOCATED BY OTHERS

STA. 11+00, 11' RT.  
 POWER POLE TO BE  
 RELOCATED BY OTHERS

STA. 11+02, RT. TO  
 STA. 14+08, RT.  
 REMOVE & HAUL:  
 229 S.Y. ASPHALT  
 71 L.F. CURB & GUTTER

STA. 14+55, 20' RT.  
 REMOVE & HAUL:  
 1- CMP CULVERT



REVISIONS	BY

**M&A**  
 Miller & Associates  
 Consulting Engineers, P.C.  
 Kearney, NE (308) 234-6466  
 McCook, NE (308) 345-3710

COMMERCE ROAD & FRONTAGE ROAD PAVING  
**REMOVALS - COMMERCE ROAD**  
 LEXINGTON, NEBRASKA



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SCALE:	
PROJECT NO.	127-A1-104
DATE:	AUGUST, 2018
FIELD BOOK	M&A DWG NO. 35316
DRAWN BY:	APRVD BY: 72H
SHEET	3

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FRONTAGE ROAD

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COMMERCE ROAD

U.S. HWY. 283

20+00.00 R.O.P. STA = 20+00.00

STA. 20+68 TO STA. 24+29  
REMOVE & HAUL:  
1473 S.Y. ASPHALT

STORM INLET  
RIM ELEV.= 2388.35  
(FL FROM W= 2387.36)  
30" INV. N & S= 2385.10

FPC- SURVEY SPIKE  
N: 39407.422  
E: 152877.541  
ELEV.= 2388.89

R.O.W. MARKER  
N: 39438.446  
E: 152877.929  
ELEV.= 2388.12

STORM MH  
RIM ELEV.= 2388.70  
8" INV. NW= 2385.20  
30" INV. N & S= 2385.20

CATCH BASIN  
GRATE ELEV.= 2386.70  
8" INV. SE= 2385.30

FPC- 5/8 REBAR  
N: 39576.837  
E: 152883.531  
ELEV.= 2388.25

WATER VALVE  
TN= 2382.52

STORM MH  
RIM ELEV.= 2388.52  
8" INV. NW= 2384.97  
30" INV. N & S= 2384.97

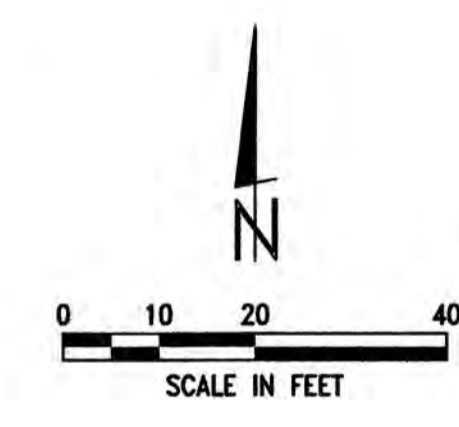
CATCH BASIN  
GRATE ELEV.= 2386.86  
8" INV. SE= 2385.26

STORM INLET  
RIM ELEV.= 2388.38  
(FL FROM W= 2387.40)  
30" INV. N & S= 2384.63

NE COR OF SW QTR. OF  
SEC 17, T-9-N, R-21-W  
N: 39710.799  
E: 152969.427  
ELEV.= 2391.42

R.O.W. MARKER  
N: 39712.547  
E: 152884.325  
ELEV.= 2387.05

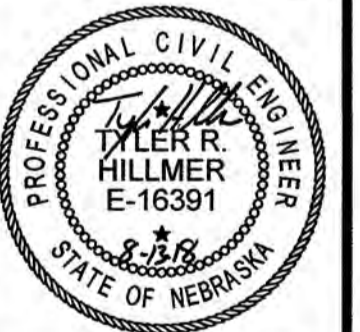
25+00.15 R.O.P. STA = 25+00.15



REVISIONS BY

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McCook, NE (308) 345-3710

COMMERCE ROAD & FRONTAGE ROAD PAVING  
**REMOVALS - FRONTAGE ROAD**  
LEXINGTON, NEBRASKA



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SCALE:  
PROJECT NO. 127-A1-104  
DATE: AUGUST, 2018  
FIELD BOOK M&A DWG NO. 35315  
DRAWN BY: TLS APRVD BY: TRH  
SHEET 4

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COMMERCE ROAD

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REVISIONS BY



STA. 15+31, 24' LT.  
 CONSTRUCT JUNCTION MANHOLE OVER  
 EXISTING 30" RCP  
 A= 3.00', X= 4.11, Y= 3.00'  
 RIM ELEV.= 2389.00  
 TOP OF 6" DECK ELEV.= 2389.00  
 EXISTING 30" INV. ELEV. (N, S)= 2384.89  
 24" RCP INV. ELEV. (W)= 2385.39  
 INSTALL 86 L.F. 24" RCP W @ 0.2% (W/  
 FLOWABLE FILL)

**EARTHWORK**

EXCAVATION:	349 C.Y.
STREET EMBANKMENT:	260 C.Y.
BORROW:	0 C.Y.
(PROVIDED BY CONTRACTOR)	
WASTE:	89 C.Y.

THE ABOVE QUANTITIES COMPUTED  
 WITHOUT ANY COMPACTION FACTOR.  
 CONTRACTOR TO FURNISH BORROW  
 MATERIAL.

STA. 12+44, 18.44' LT.  
 CONSTRUCT JUNCTION MANHOLE  
 A= 3.00', X= 2.66', Y= 3.00'  
 RIM ELEV.= 2389.45  
 TOP OF 6" DECK ELEV.= 2389.45  
 15" INV. ELEV. (N, E, W) = 2386.79  
 INSTALL 8 L.F. 15" HDPE N @ 0.24%  
 INSTALL (AT 18.44' LT.)  
 84 L.F. 15" RCP W @ 0.24%  
 INSTALL (AT 26.44' LT.)  
 1- TEMPORARY PIPE PLUG ON 15" HDPE

STA. 13+84, 18.44' LT.  
 CONSTRUCT JUNCTION MANHOLE  
 A= 3.00', X= 2.35', Y= 3.00'  
 RIM ELEV.= 2388.80  
 TOP OF 6" DECK ELEV.= 2388.80  
 15" INV. ELEV. (N, E, W) = 2386.45  
 INSTALL 8 L.F. 15" HDPE N @ 0.24%  
 INSTALL (AT 18.44' LT.)  
 140 L.F. 15" RCP W @ 0.24%  
 INSTALL (AT 26.44' LT.)  
 1- TEMPORARY PIPE PLUG ON 15" HDPE

STA. 11+00 TO STA. 15+30  
 CONSTRUCT:  
 1909 S.Y. 6" THICK  
 CONCRETE PAVEMENT  
 777 L.F. CURB & GUTTER

STA. 11+00  
 CONSTRUCT 30 L.F.  
 CONCRETE HEADER

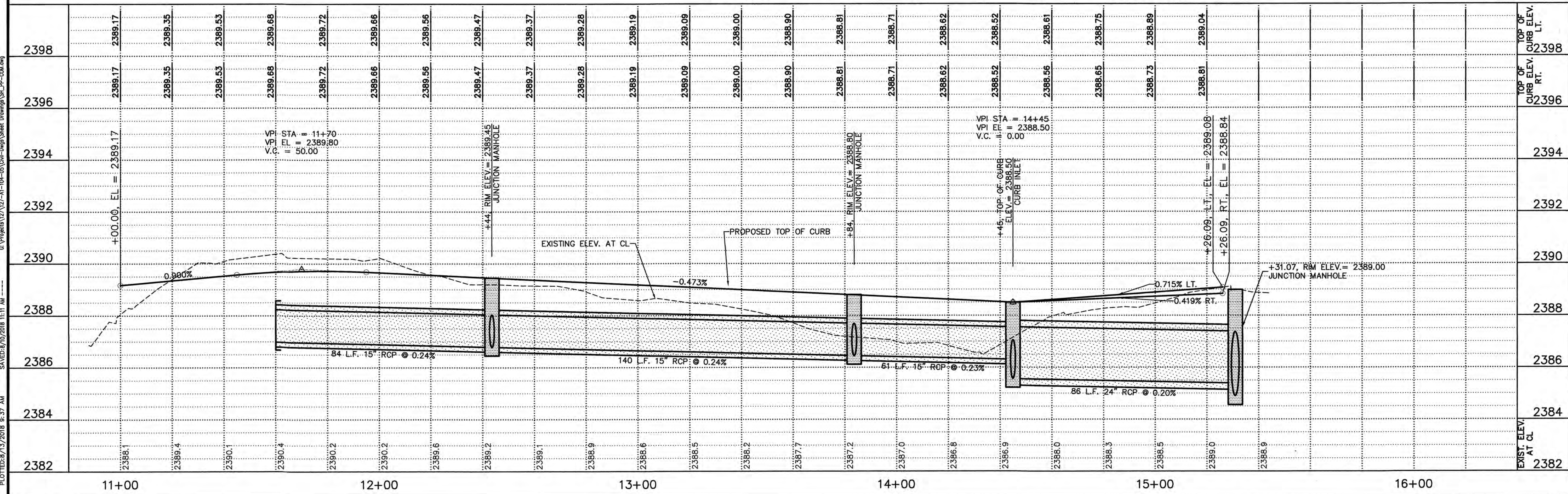
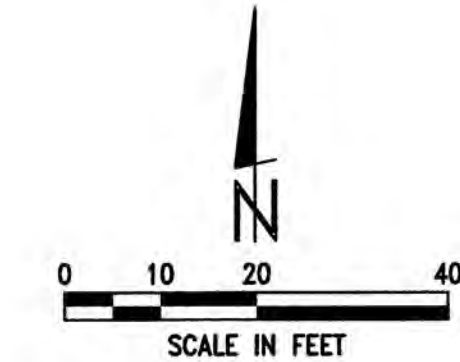
STA. 11+60, 18.44' LT.  
 INSTALL TEMPORARY PIPE  
 PLUG ON 15" RCP  
 15" INV. ELEV. AT PLUG  
 = 2386.99

STA. 11+42, 15' RT. TO  
 STA. 14+67, 56' RT.  
 CONSTRUCT:  
 1791 S.F. 6" THICK SIDEWALK  
 WITH 6" HIGH TRAFFIC  
 BARRIER CURB (SEE DETAILS)

STA. 14+45, 20.58' LT.  
 CONSTRUCT CURB INLET W/ 4' THROAT  
 A= 5.17', X= 2.94', Y= 4.00'  
 TOP OF CURB ELEV.= 2388.50  
 24" RCP INV. ELEV. (E)= 2385.56  
 18" RCP INV. ELEV. (S)= 2385.56  
 15" RCP INV. ELEV. (W)= 2386.31  
 INSTALL 38 L.F. 18" RCP S @ 0.2%  
 (W/ FLOWABLE FILL)  
 INSTALL (AT 18.44' LT.) 61 L.F. 15"  
 RCP W @ 0.24%

STA. 14+45, 16.5' RT.  
 CONSTRUCT CURB INLET W/ 8' THROAT  
 A= 3.00', X= 2.86', Y= 8.00'  
 TOP OF CURB ELEV.= 2388.50  
 18" RCP INV. ELEV.= 2385.63

STA. 13+91, 20' RT. TO  
 STA. 14+57, 53' RT  
 CONSTRUCT:  
 110 S.Y. 6" THICK ASPHALT PARKING LOT



COMMERCE ROAD & FRONTAGE ROAD PAVING  
**PLAN & PROFILE - PAVING**  
 LEXINGTON, NEBRASKA



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 PROJECT NO.  
 127-A1-104  
 DATE:  
 AUGUST, 2018  
 FIELD BOOK  
 M&A DWG NO.  
 35314  
 DRAWN BY:  
 TJS  
 APPROV BY:  
 TRH  
 SHEET  
**5**

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FRONTAGE ROAD

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STA. 20+68 TO STA. 21+58  
CONSTRUCT:  
402 S.Y. 6" THICK  
CONCRETE PAVEMENT  
79 L.F. CURB & GUTTER

STA. 22+53 TO STA. 24+29  
CONSTRUCT:  
642 S.Y. 6" THICK  
CONCRETE PAVEMENT  
354 L.F. CURB & GUTTER

STA. 24+11  
START 6" CURB  
TAPER DOWN  
FROM FULL CURB  
TO NO CURB  
AROUND RADIUS

STA. 20+69, 14' RT TO STA. 21+38, 25' RT.  
TAPER DOWN FROM CURB TO NO CURB  
MATCH ELEV. OF EXISTING CURB & GUTTER

EARTHWORK	
EXCAVATION:	163 C.Y.
STREET EMBANKMENT:	140 C.Y.
BORROW:	0 C.Y.
(PROVIDED BY CONTRACTOR)	
WASTE:	23 C.Y.

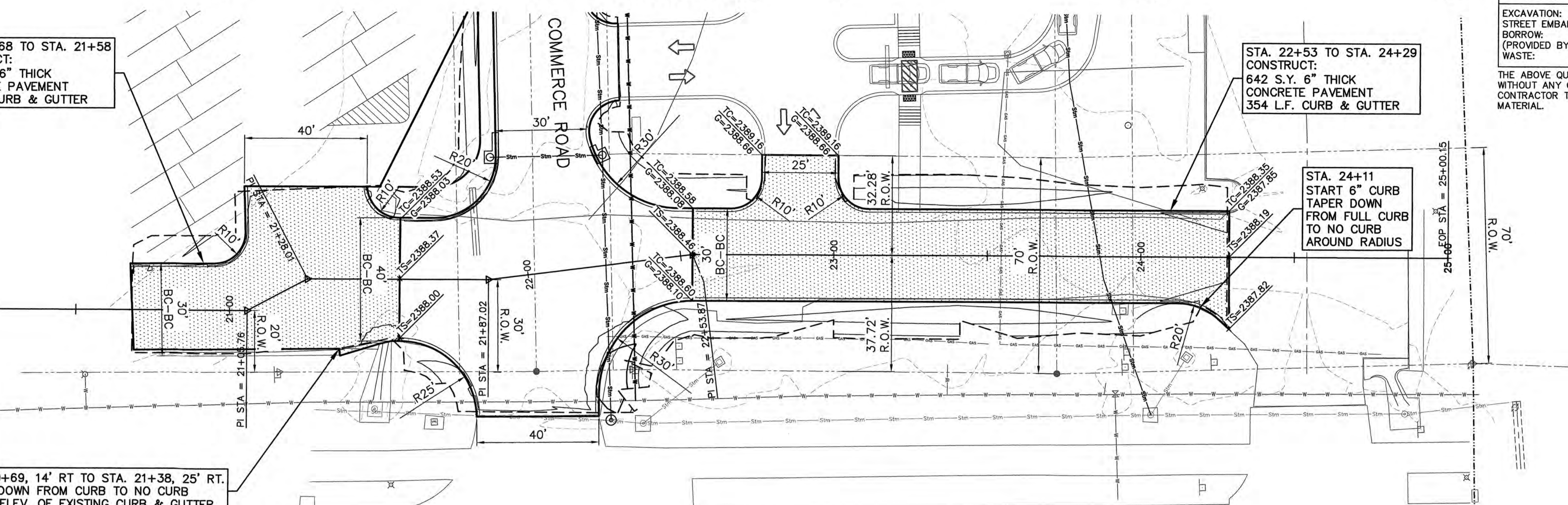
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WITHOUT ANY COMPACTION FACTOR.  
CONTRACTOR TO FURNISH BORROW  
MATERIAL.

REVISIONS	BY

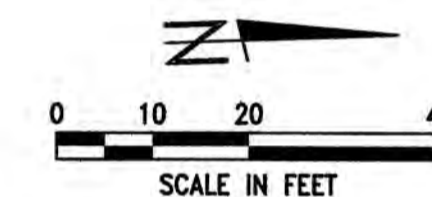
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Consulting Engineers, P.C.  
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20+00 BOP STA = 20+00.00

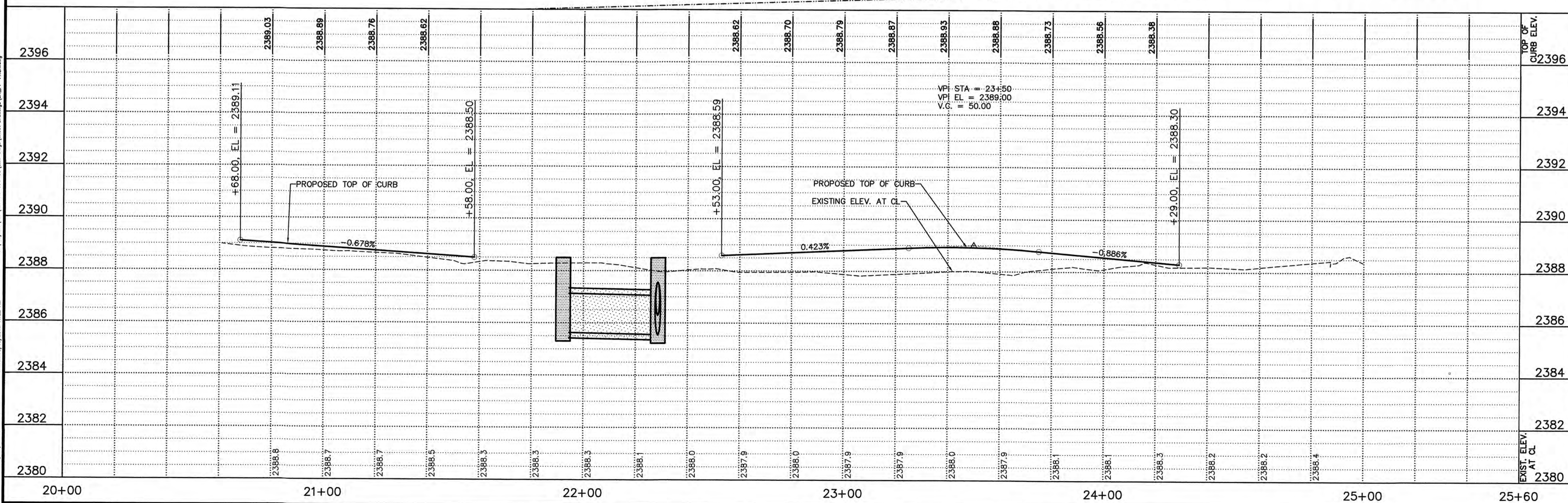
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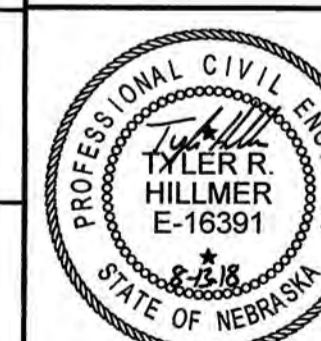
U.S. HWY. 283



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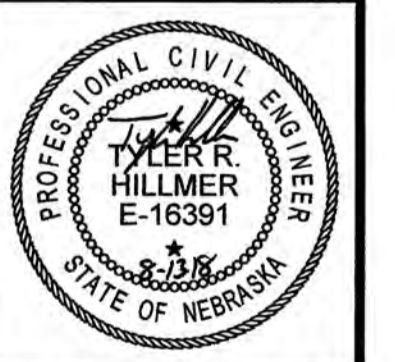
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**PLAN & PROFILE - PAVING**  
LEXINGTON, NEBRASKA



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DATE:	AUGUST, 2018
FIELD BOOK	M&A DWG NO. 35313
DRAWN BY:	TLS
APR'D BY:	TRJ
SHEET	6

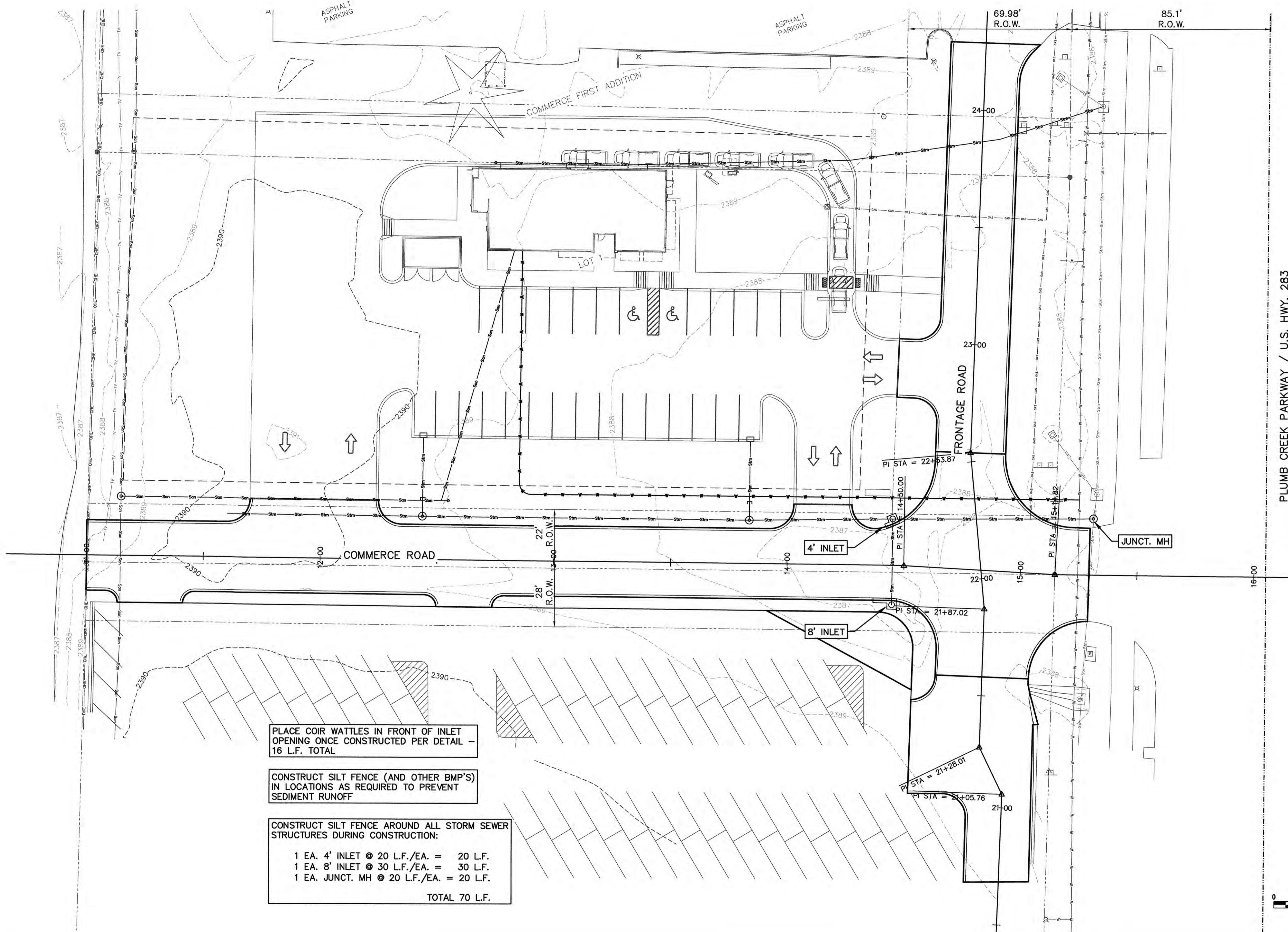
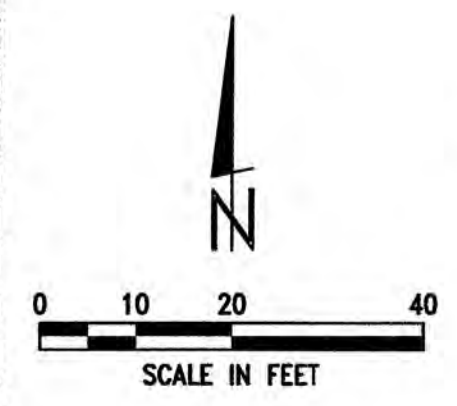
COMMERCE ROAD & FRONTAGE ROAD PAVING  
**EROSION CONTROL PLAN**  
 LEXINGTON, NEBRASKA



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 DATE: AUGUST, 2018  
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 DRAWN BY: TJS APRVD BY: TRH  
 SHEET 7



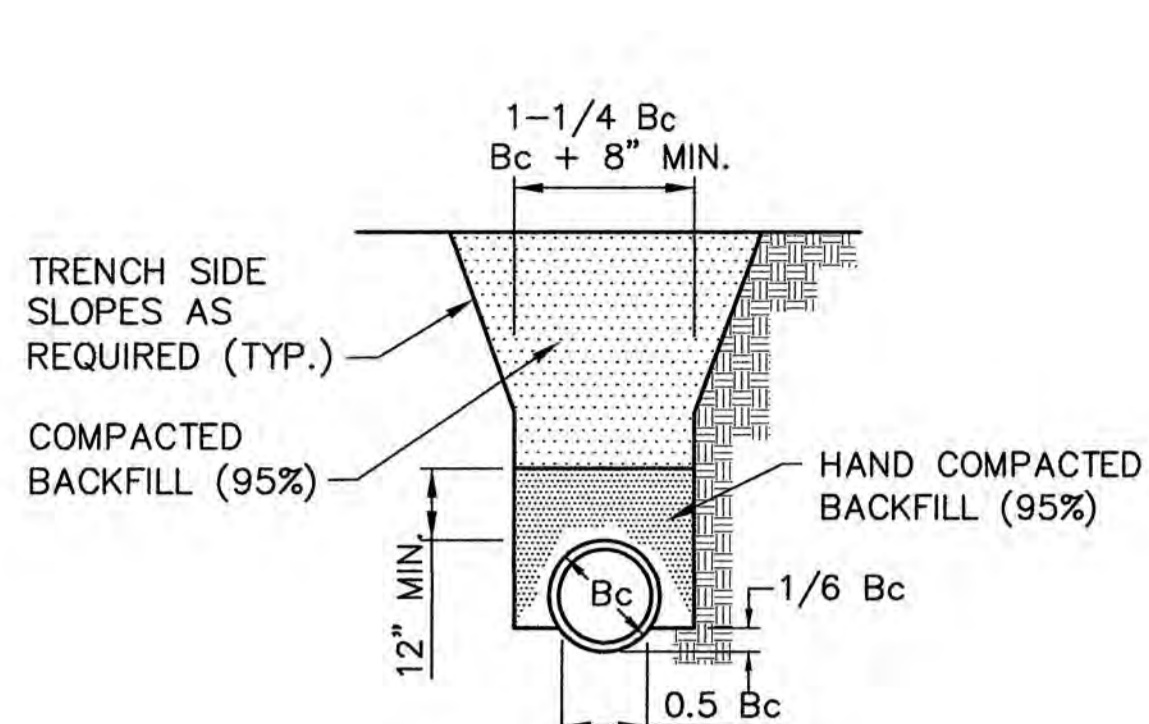
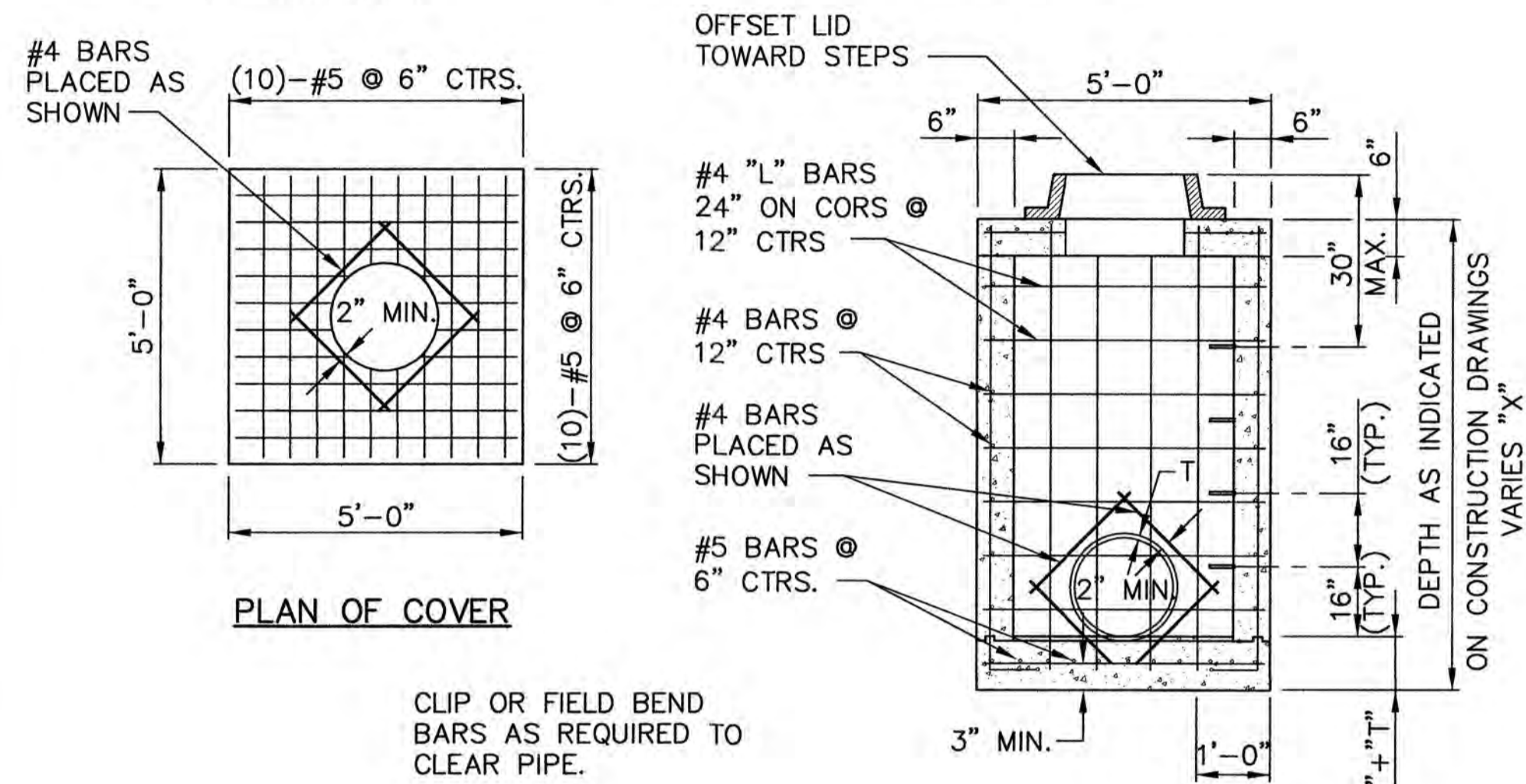
PLACE COIR WATTLES IN FRONT OF INLET OPENING ONCE CONSTRUCTED PER DETAIL - 16 L.F. TOTAL

CONSTRUCT SILT FENCE (AND OTHER BMP'S) IN LOCATIONS AS REQUIRED TO PREVENT SEDIMENT RUNOFF

- CONSTRUCT SILT FENCE AROUND ALL STORM SEWER STRUCTURES DURING CONSTRUCTION:
- 1 EA. 4' INLET @ 20 L.F./EA. = 20 L.F.
  - 1 EA. 8' INLET @ 30 L.F./EA. = 30 L.F.
  - 1 EA. JUNCT. MH @ 20 L.F./EA. = 20 L.F.
- TOTAL 70 L.F.

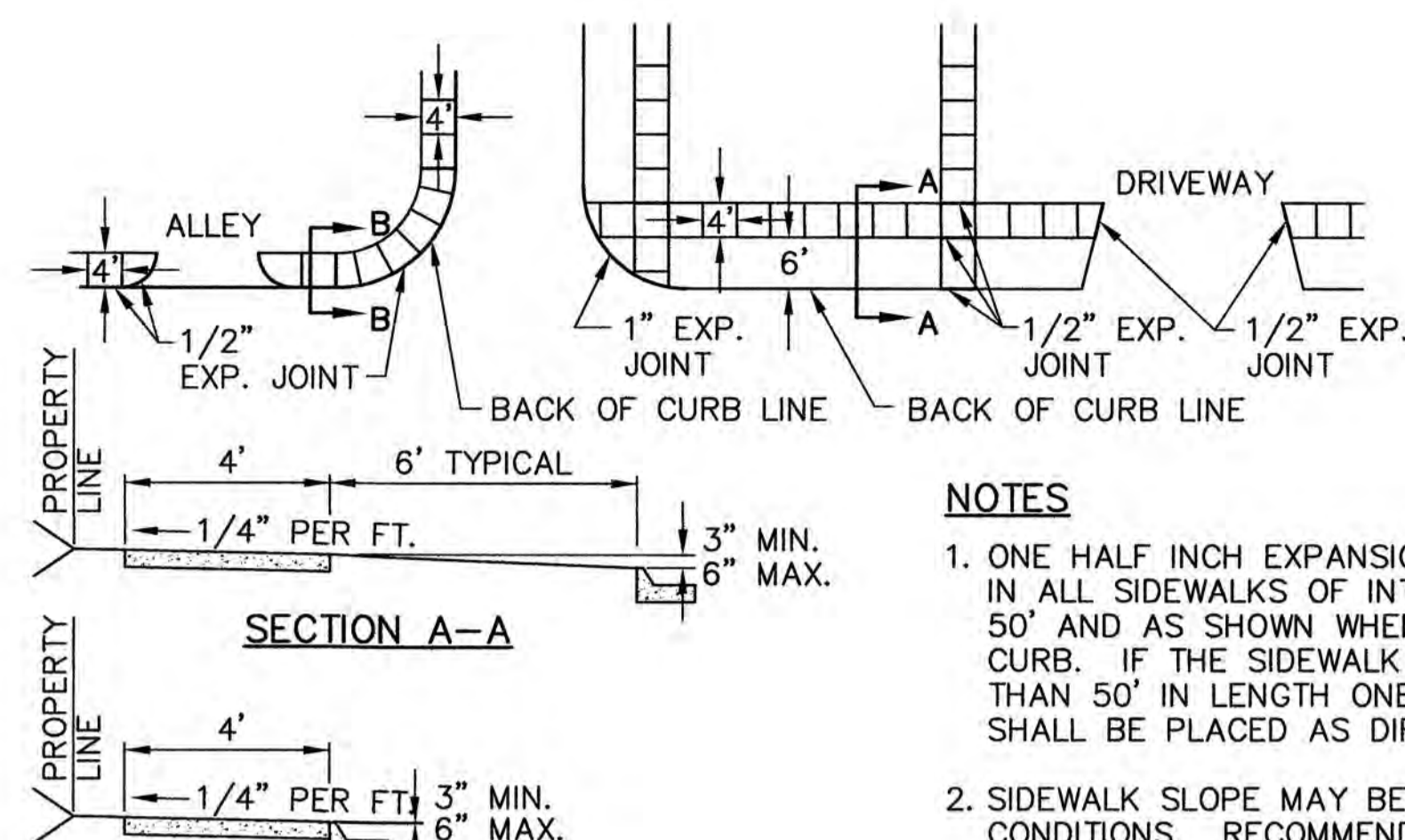
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- NOTES:**
1. MAXIMUM SIZE PIPE TO BE USED - 36" I.D.
  2. TOTAL DEPTH OF MANHOLE SHALL BE AS SPECIFIED ON CONSTRUCTION DRAWINGS.
  3. MANHOLE RING AND COVER SHALL BE DEETER 1030, OR APPROVED EQUAL, IN ROAD-WAY OR DEETER 2000, OR APPROVED EQUAL, FOR OTHER CONDITIONS.
  4. MINIMUM COVER SHALL BE 2" EXCEPT AS SHOWN, FROM THE FACE OF CONCRETE TO SURFACE OF ANY REINFORCING BARS.
  5. CAST IRON STEP SHALL BE INSTALLED WHEN THE "X" DIMENSION IS 4'-6" OR GREATER.

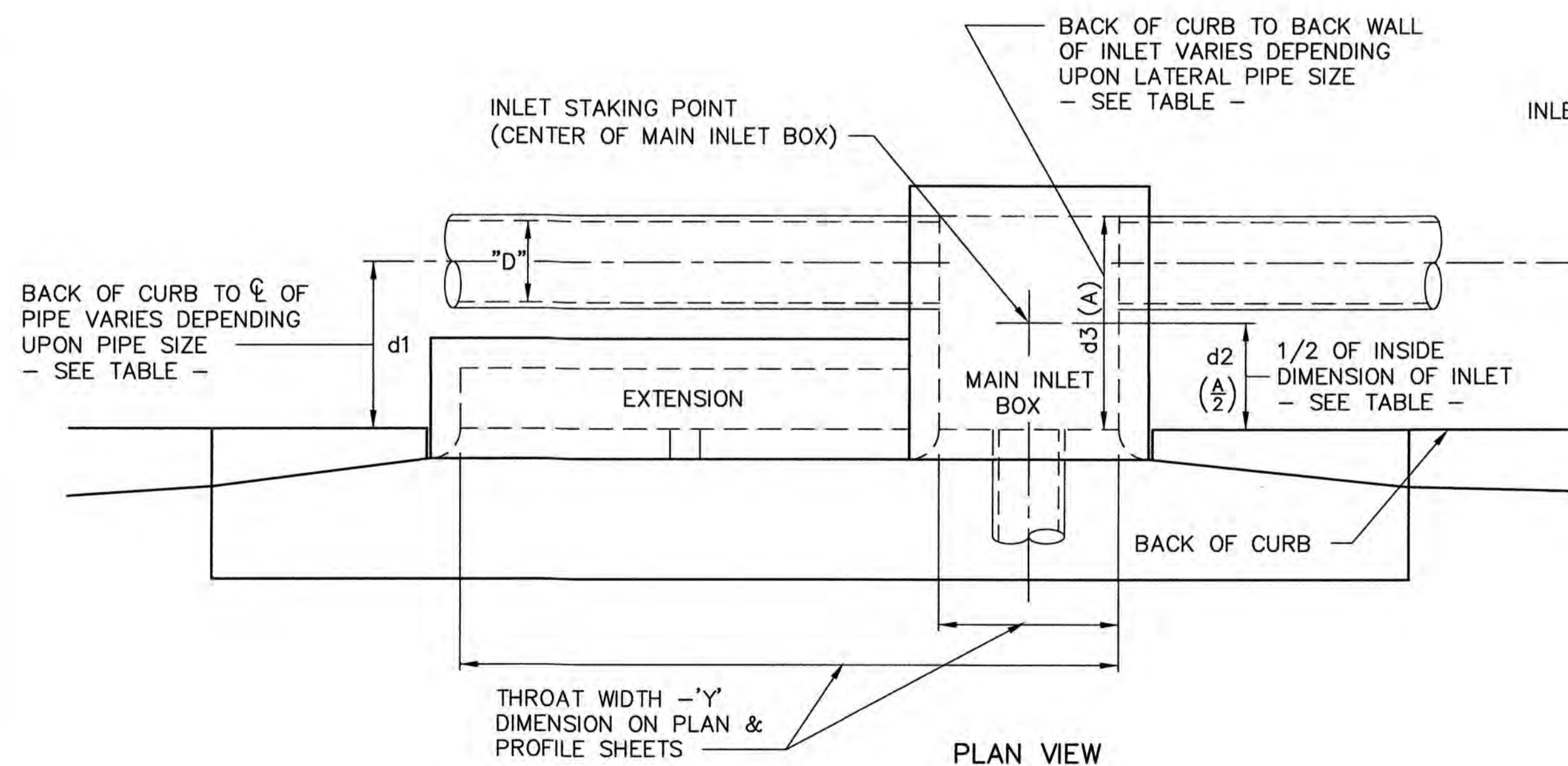
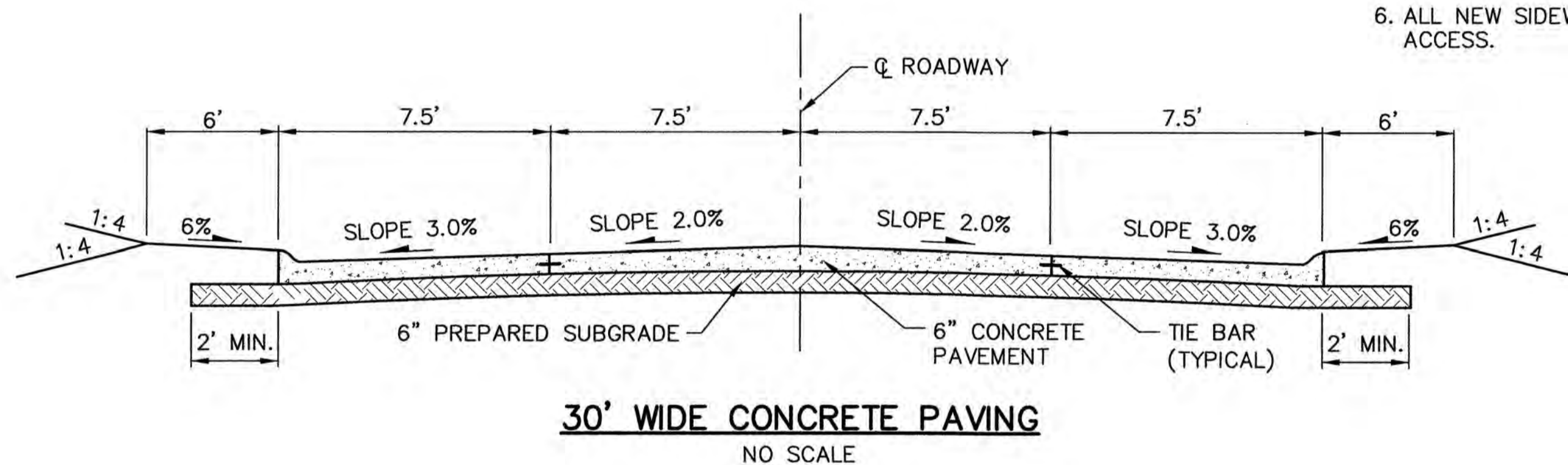


**LEGEND**  
 Bc = OUTSIDE DIAMETER  
 H = BACKFILL COVER ABOVE TOP OF PIPE  
 D = INSIDE DIAMETER

**SHAPED SUBGRADE**  
**CLASS "C" BEDDING (FOR STORMWATER)**  
 NO SCALE



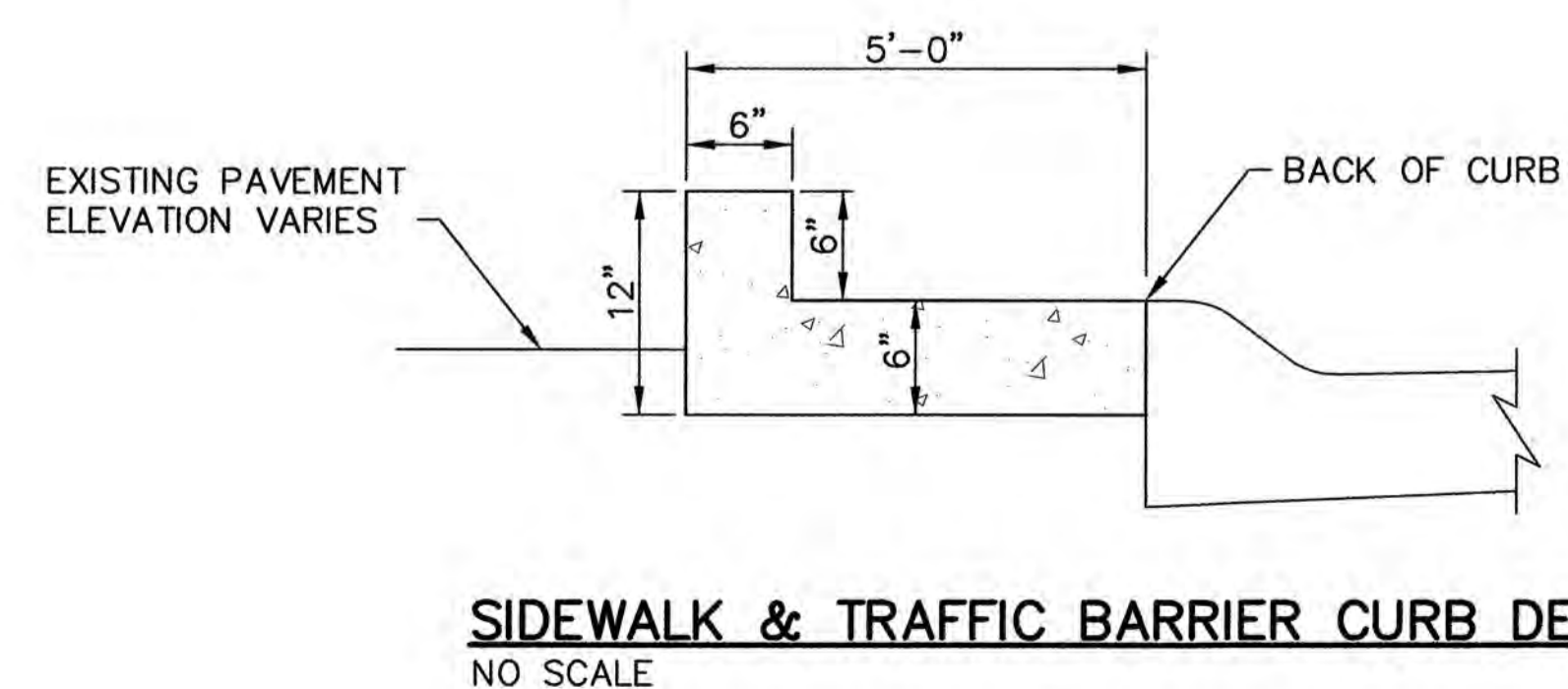
- NOTES**
1. ONE HALF INCH EXPANSION JOINTS SHALL BE PLACED IN ALL SIDEWALKS OF INTERVALS OF NOT MORE THAN 50' AND AS SHOWN WHERE SIDEWALKS BUTT AGAINST CURB. IF THE SIDEWALK TO BE CONSTRUCTED IS LESS THAN 50' IN LENGTH ONE SUCH EXPANSION JOINT SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
  2. SIDEWALK SLOPE MAY BE VARIED TO SUIT EXISTING CONDITIONS. RECOMMENDED SLOPE TO BE 1/4"/FT. VARIATIONS TO SUIT EXTREME CONDITIONS TO BE AS DIRECTED BY THE ENGINEER. MAX. ALLOWABLE CROSS SECTION SLOPE = 1/2" PER FT. MAX. ALLOWABLE LONGITUDINAL SLOPE = 1" PER FT.
  3. ALL SIDEWALKS 4 FEET OR MORE IN WIDTH SHALL BE SCORED IN PANELS NO GREATER THAN 4' SQUARE.
  4. NO SIDEWALK REPAIR SHALL BE LESS THAN ONE COMPLETE PANEL.
  5. SIDEWALK INSTALLED ADJACENT TO CURB SHALL BE INSTALLED 0.02' HIGHER THAN TOP OF CURB.
  6. ALL NEW SIDEWALK SHALL HAVE THE PROPER HANDICAP ACCESS.



INLET DEPTH = 'X' DIMENSION ON PLAN & PROFILE SHEETS  
 d3 = 'A' DIMENSION ON PLAN & PROFILE SHEETS  
 d3 = 3.00' IF NO LONGITUDINAL PIPE  
 d2 = 1.50' IF NO LONGITUDINAL PIPE  
 d1 = CENTER OF LONGITUDINAL PIPE BEHIND BACK OF CURB.

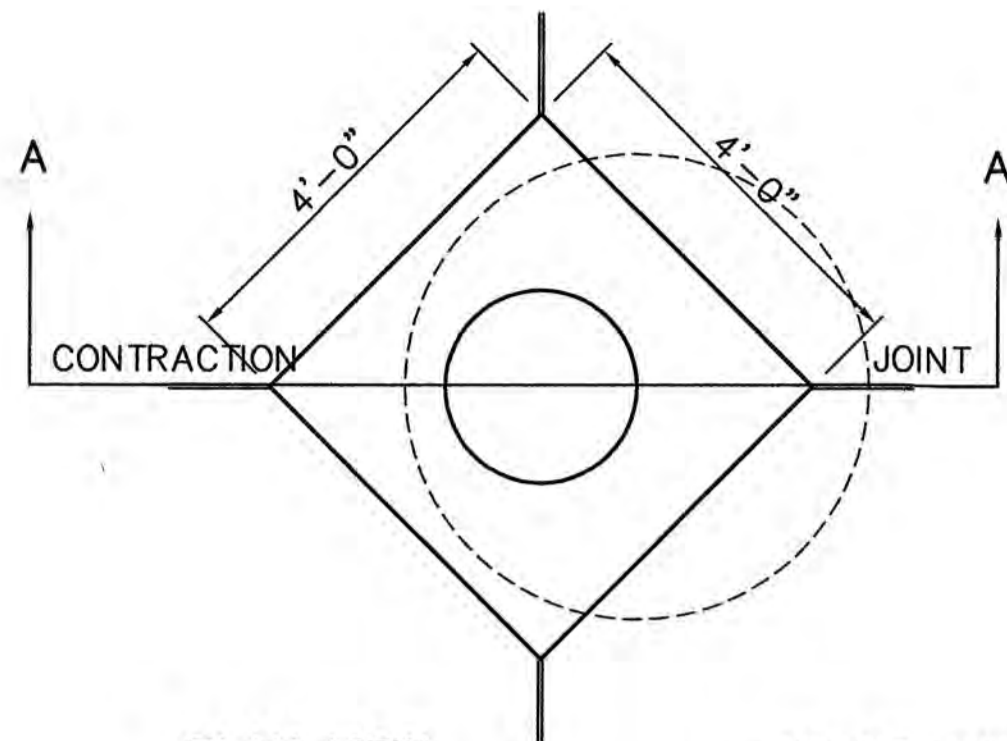
DIA. OF PIPE PARALLEL TO ROADWAY "D"	d1	d2 (A/2)	d3 (A)
12"	3.33'	2.00'	4.00'
15"	3.44'	2.12'	4.25'
18"	3.62'	2.29'	4.58'
21"	3.82'	2.46'	4.92'
24"	3.92'	2.58'	5.17'
27"	4.10'	2.75'	5.50'
30"	4.21'	2.87'	5.75'
36"	4.50'	3.17'	6.33'
42"	4.80'	3.46'	6.92'
48"	5.08'	3.75'	7.50'
54"	5.37'	4.04'	8.08'
60"	5.67'	4.33'	8.67'
66"	5.96'	4.62'	9.25'

d3 = 2(d2)

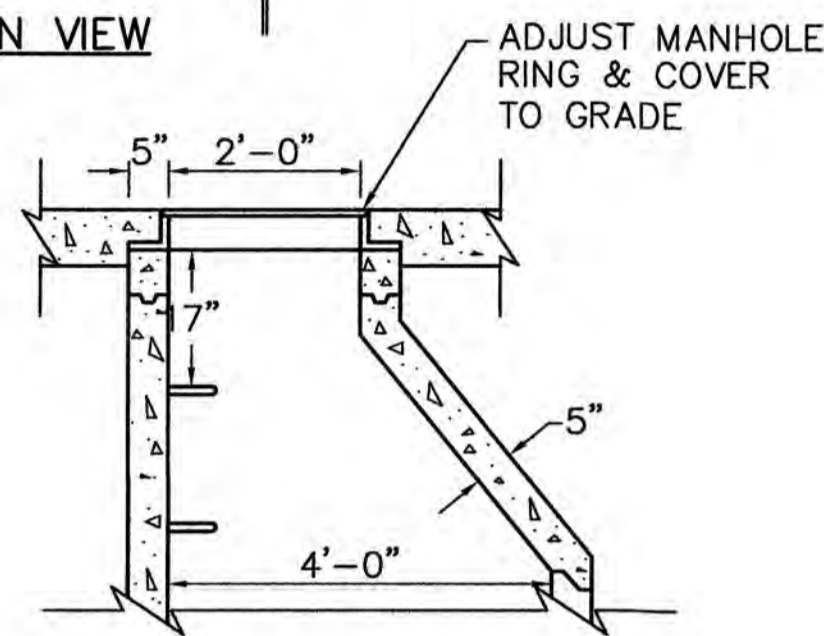






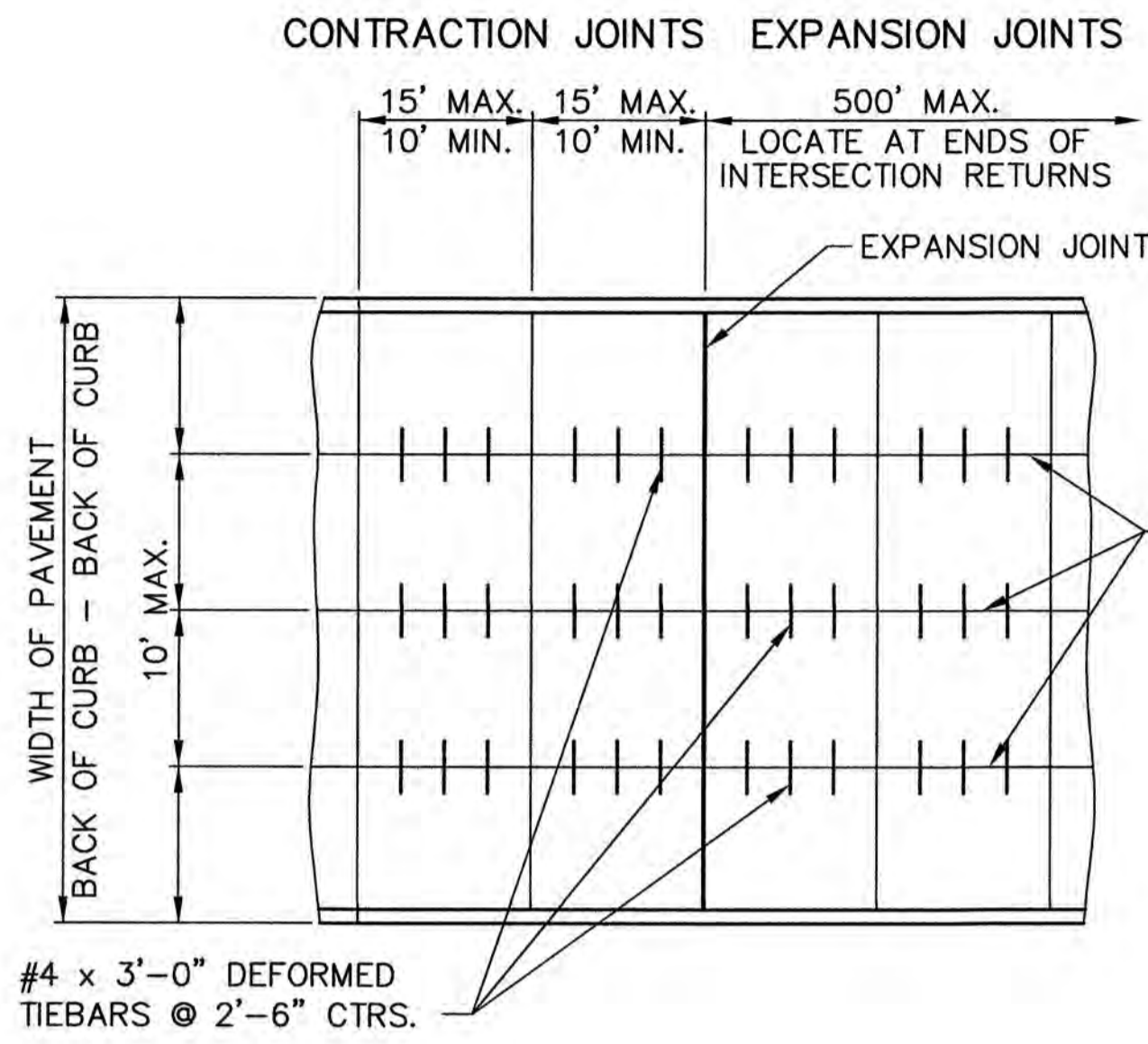


PLAN VIEW

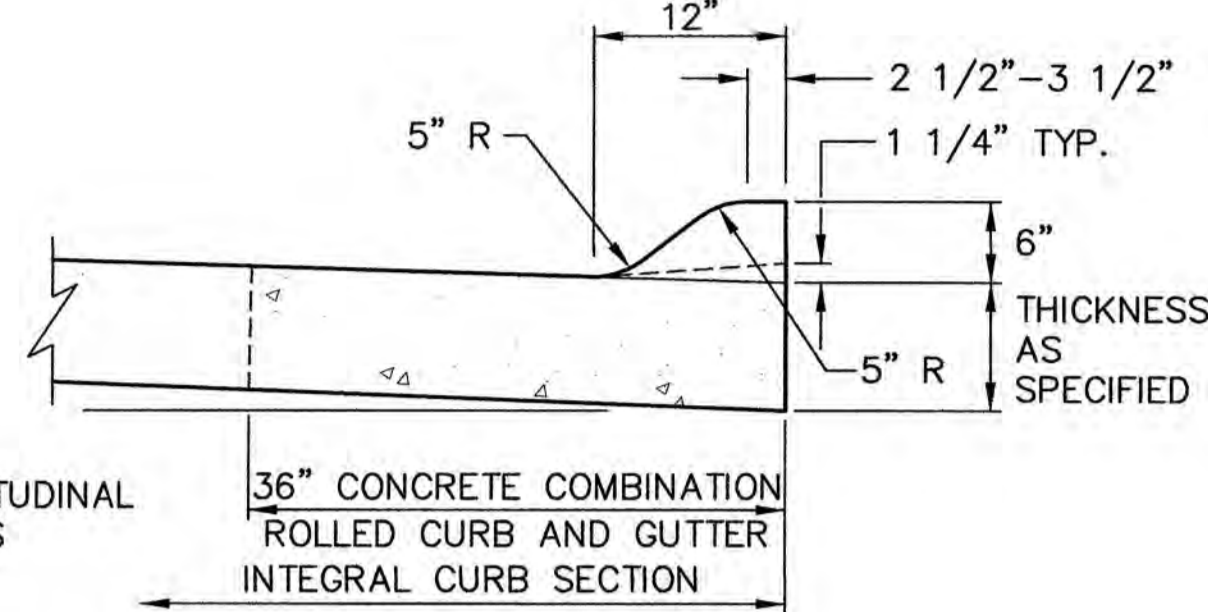


SECTION A-A

STANDARD MANHOLE ADJUSTMENT DETAIL  
NO SCALE

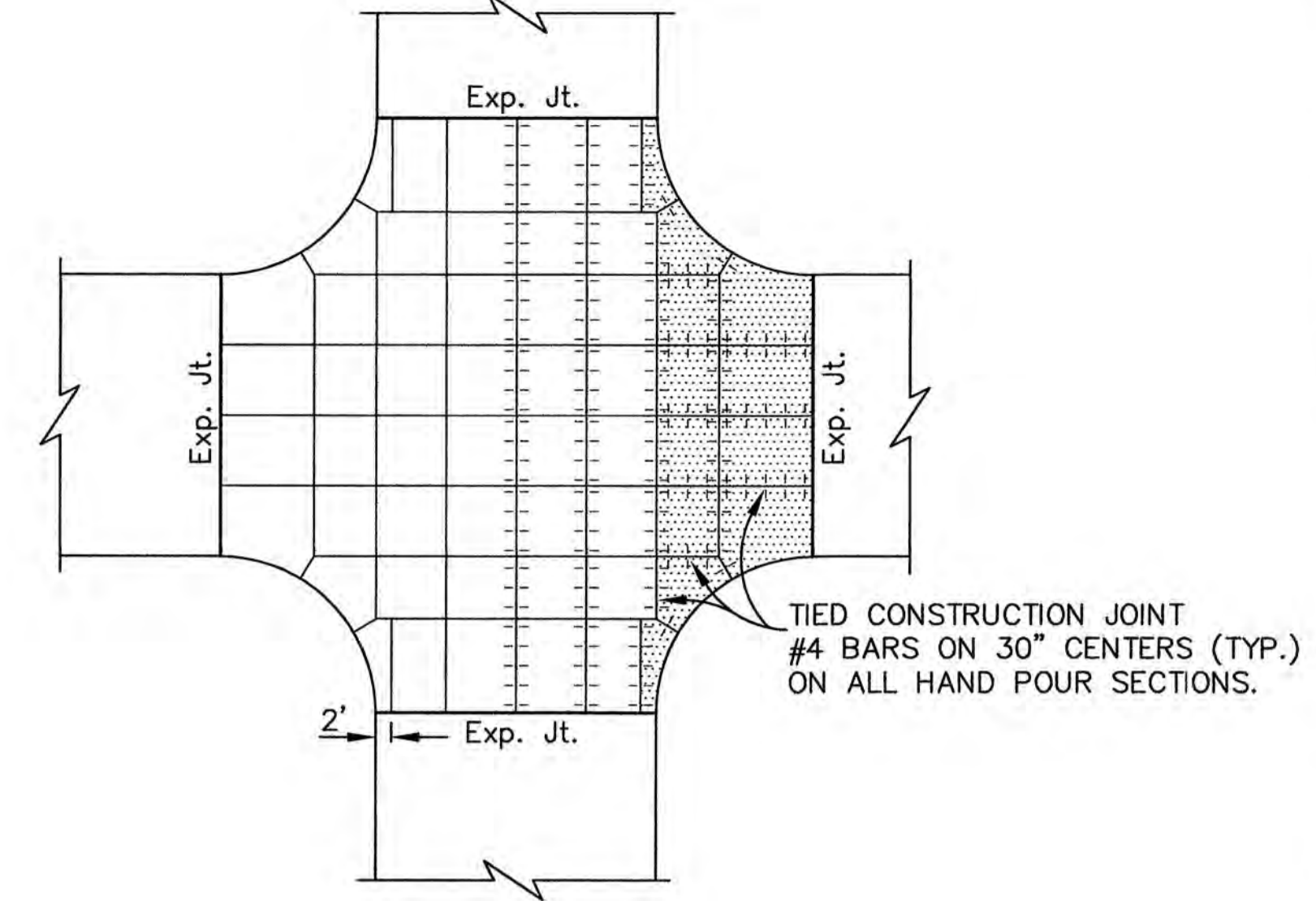


REINFORCING DETAIL  
NO SCALE

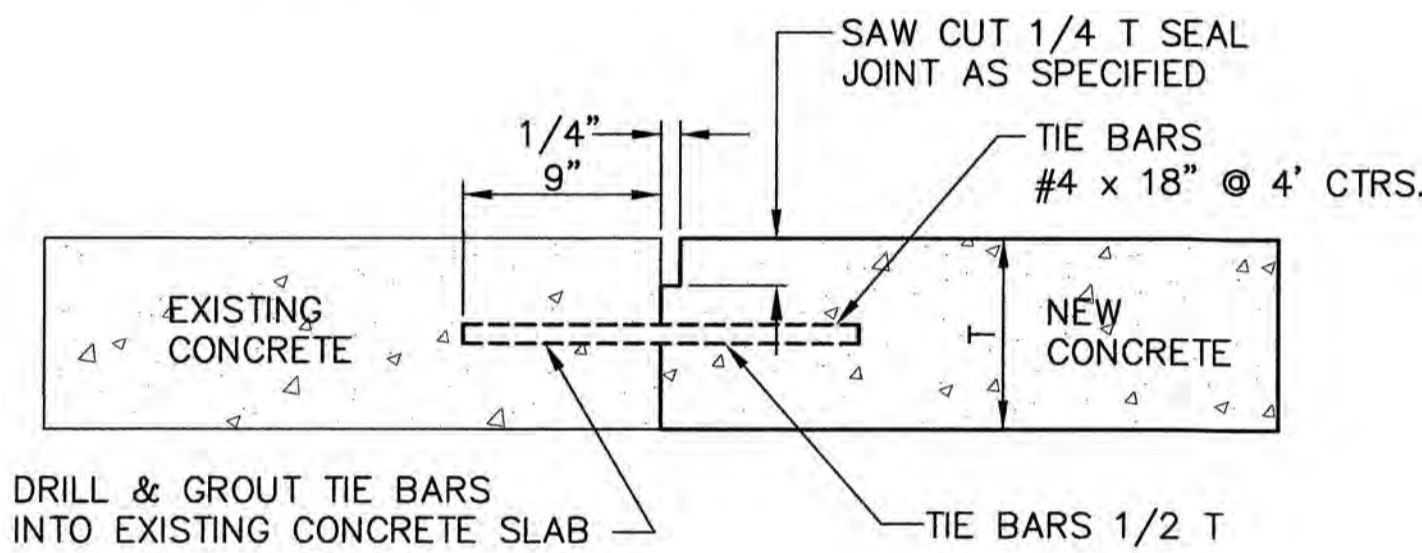


STANDARD 6" CURB SECTION  
NO SCALE

NOTE:  
EXTREME CARE SHALL BE USED IN FINISH WORK TO ELIMINATE "LOW SPOTS" AND "POCKETS" IN FLAT GRADES

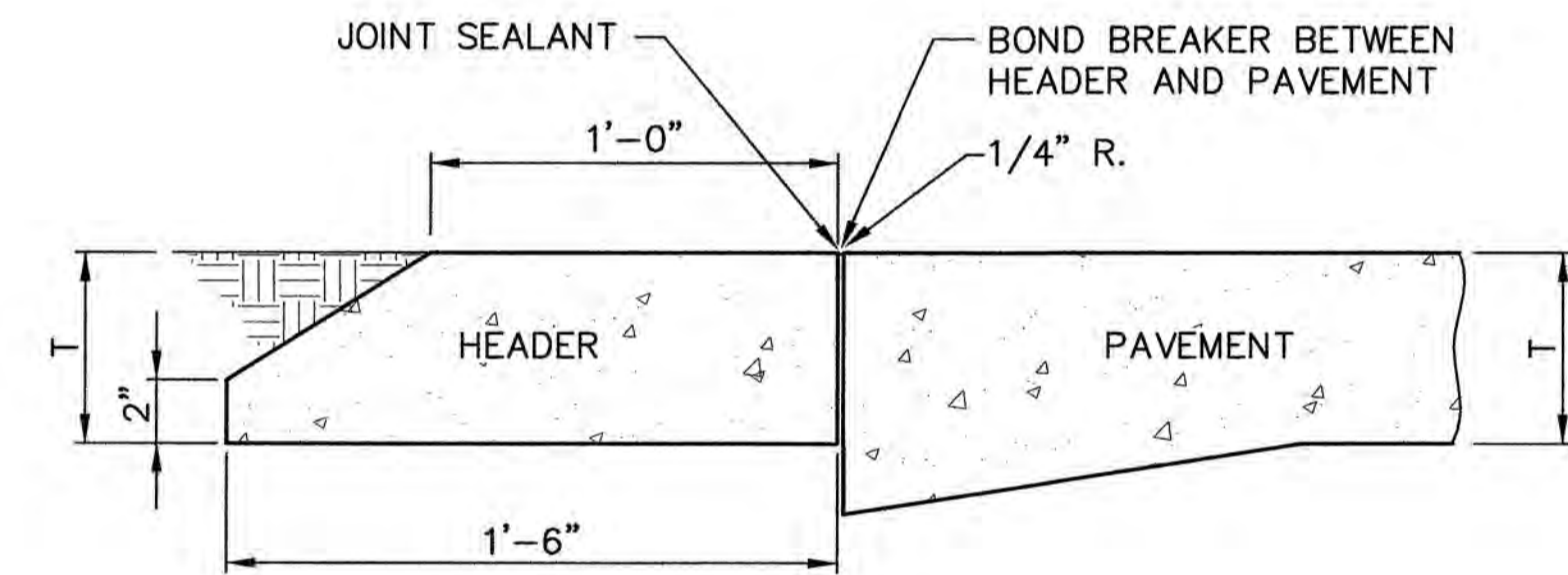


TYPICAL INTERSECTION  
JOINT PATTERN  
NO SCALE

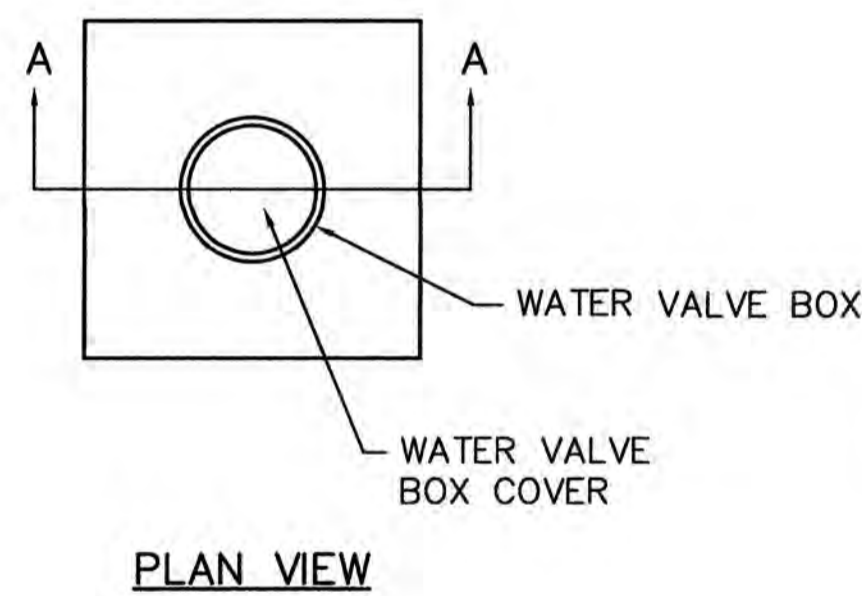


TIE BAR DETAIL  
NO SCALE

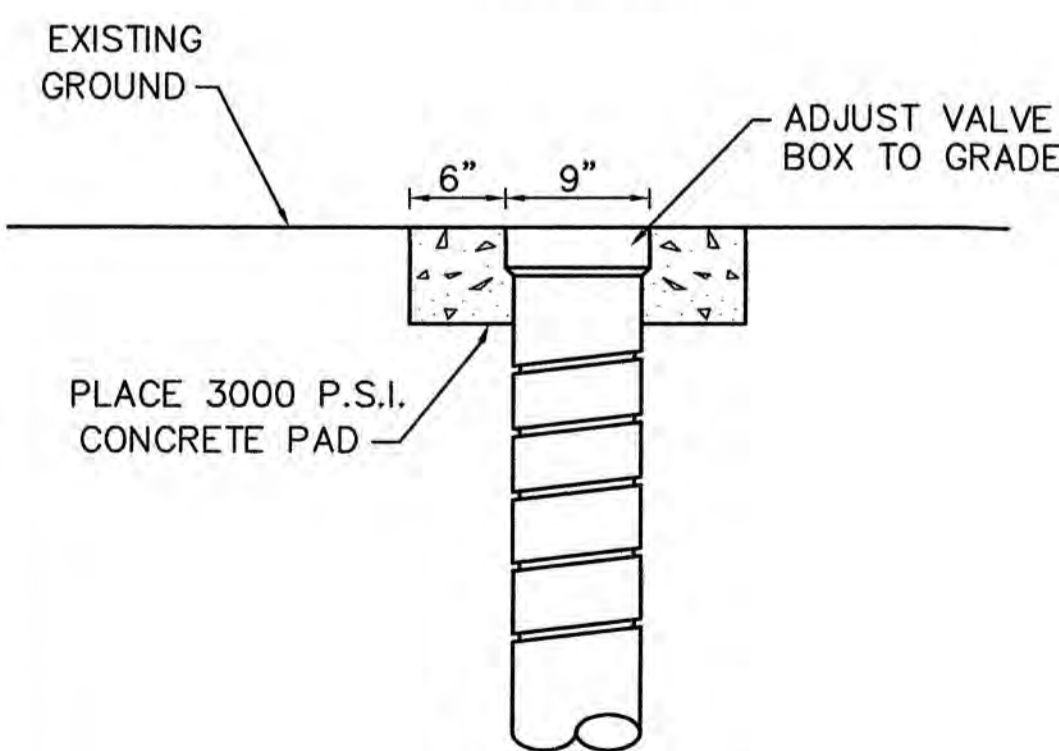
T = THICKNESS OF NEW CONCRETE



TYPICAL HEADER  
NO SCALE

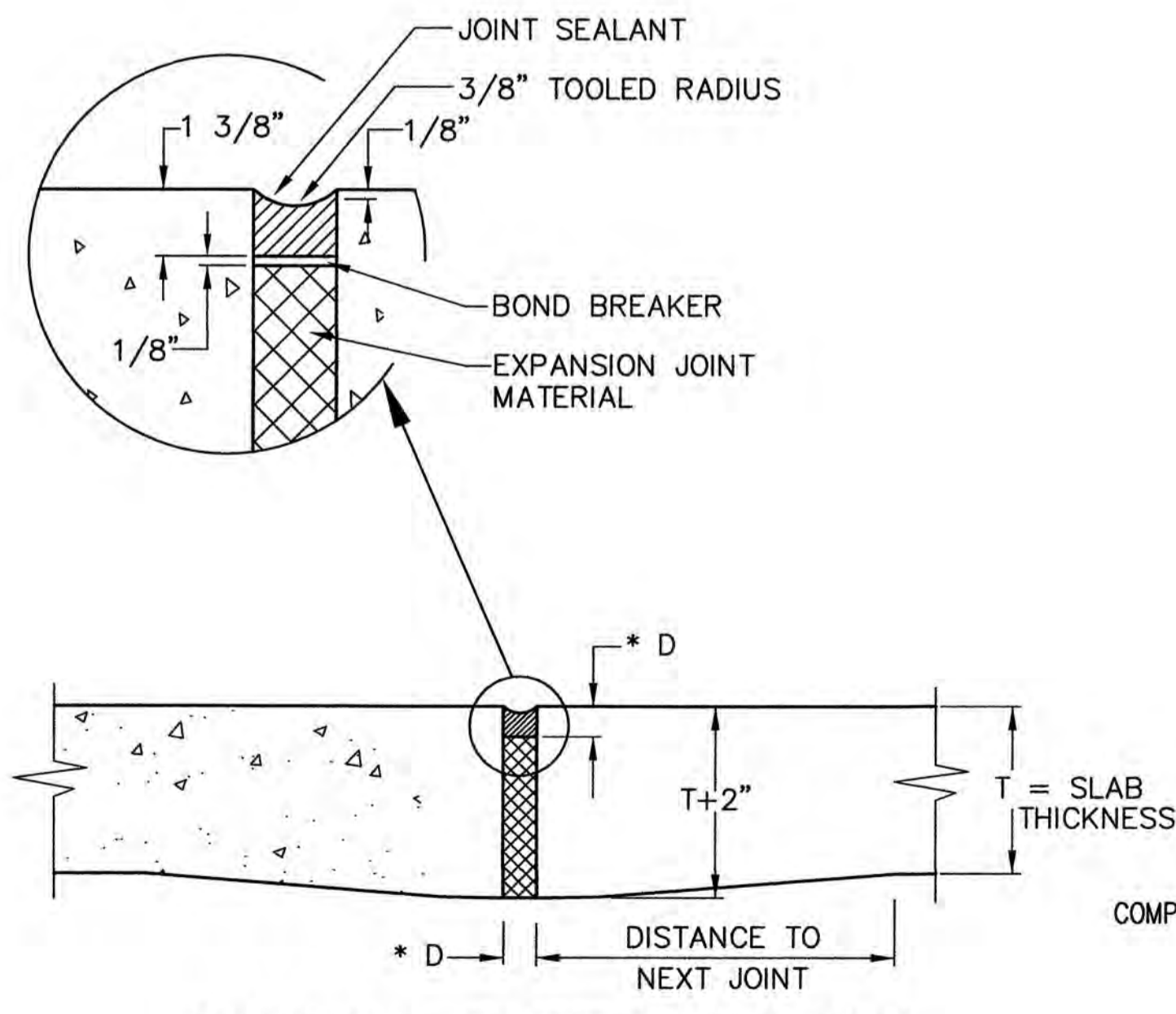


PLAN VIEW



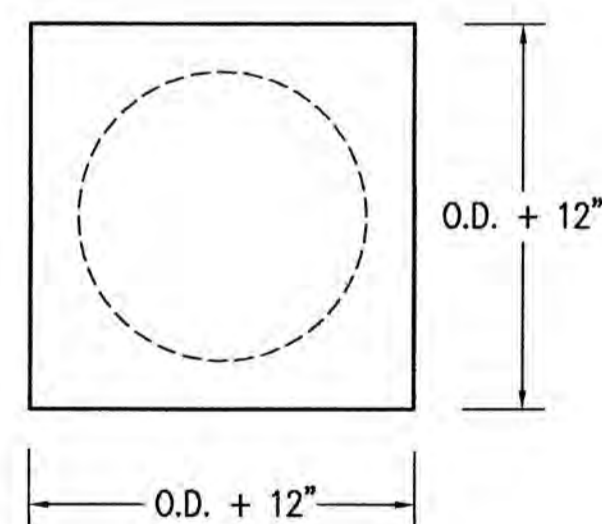
SECTION A-A

STANDARD VALVE BOX ADJUSTMENT DETAIL  
NO SCALE

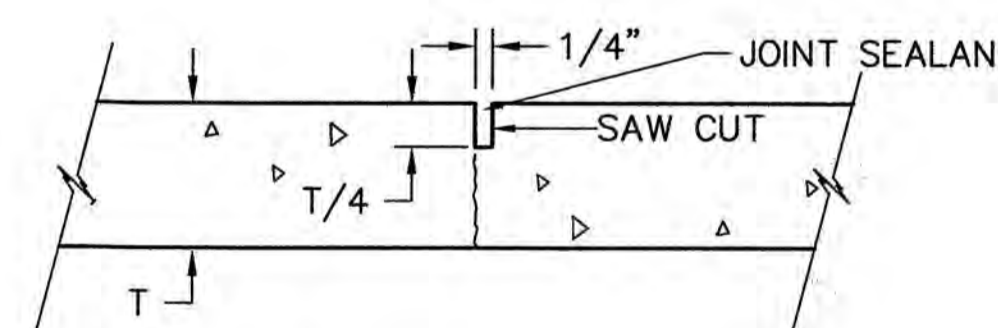


THICKENED EDGE EXPANSION JOINT  
NO SCALE

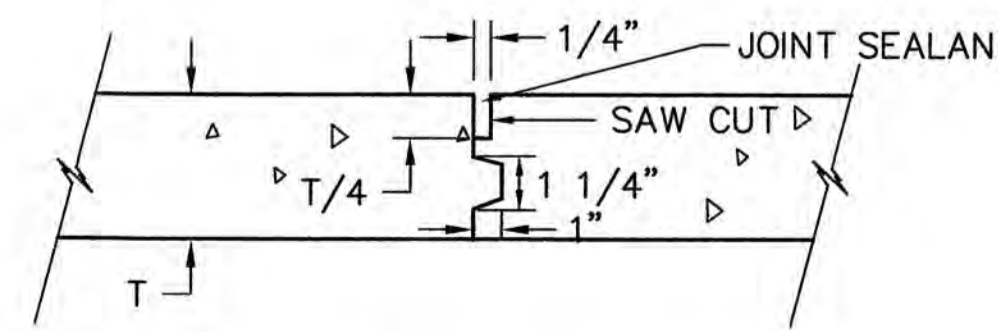
\* DIMENSION D = 1" WHERE T = 6" PAVEMENT  
\* DIMENSION D = 2" WHERE T = 8" PAVEMENT



TEMPORARY PIPE PLUG  
NO SCALE

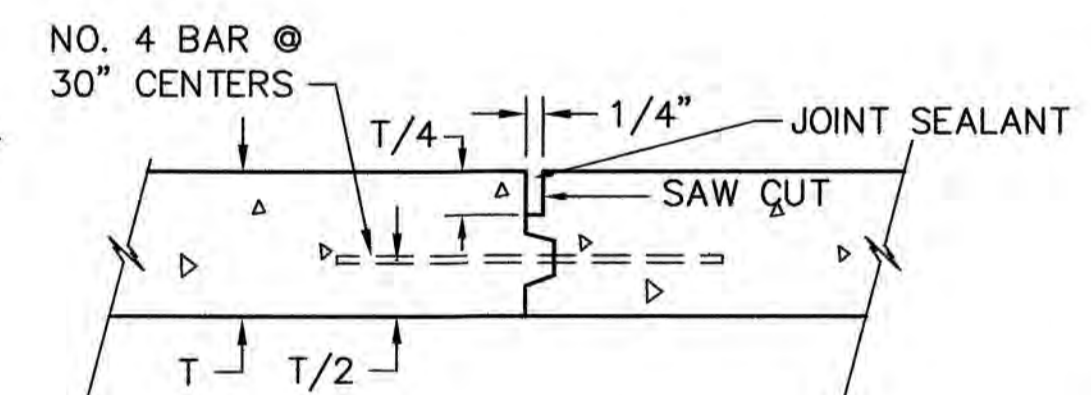


SAWN CONTRACTION JOINT

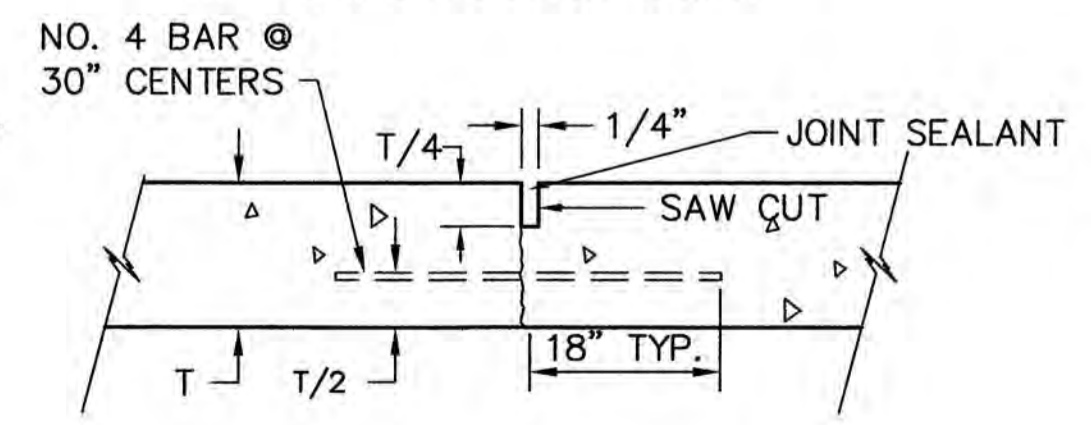


KEYED CONSTRUCTION JOINT

CONCRETE JOINT DETAIL  
NO SCALE



TIED CONSTRUCTION JOINT

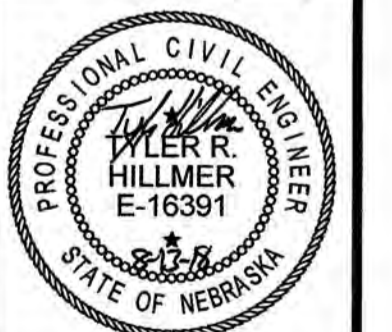


TIED CONTRACTION JOINT

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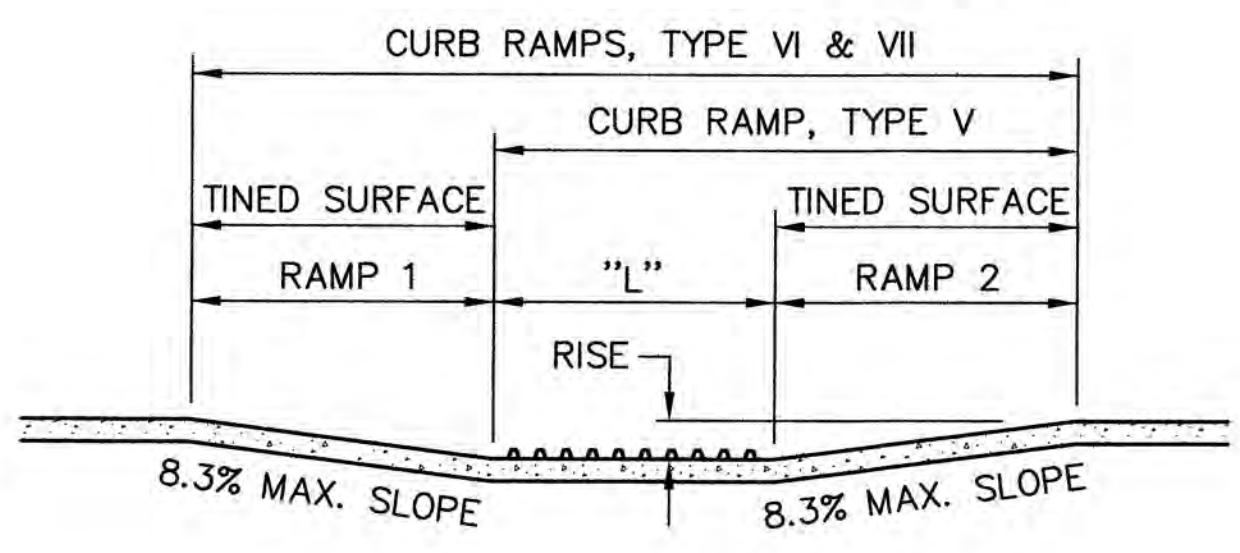
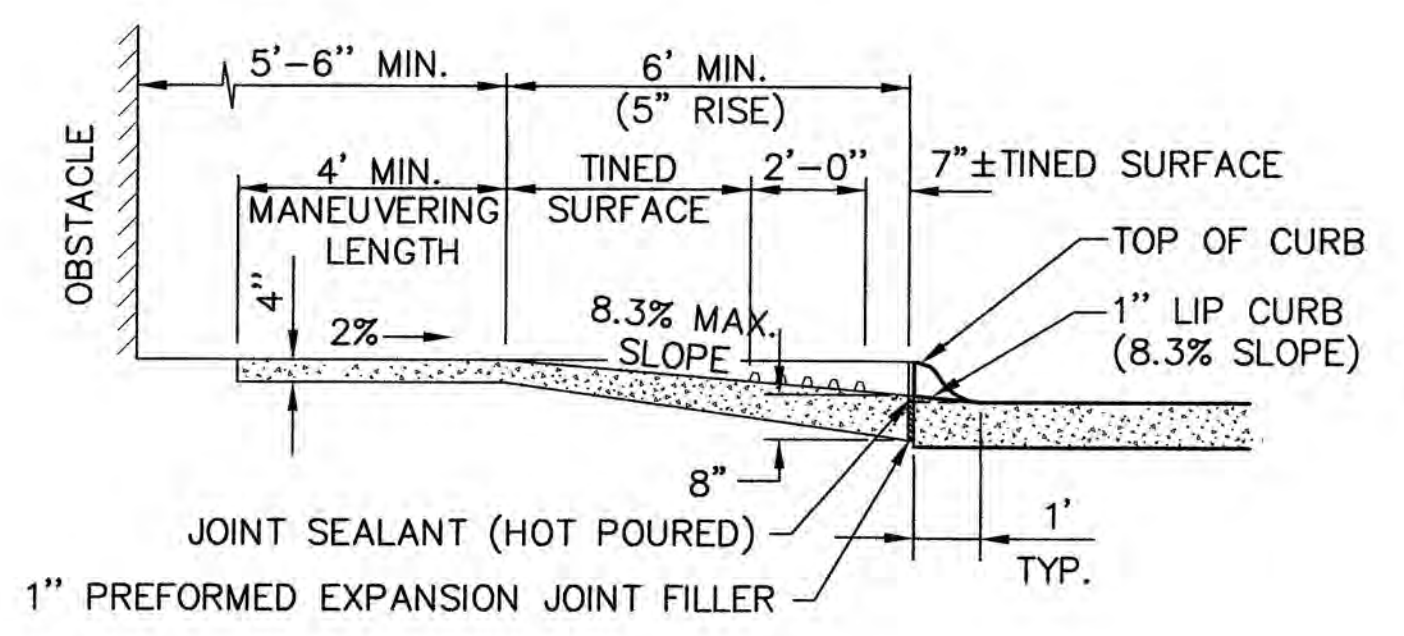
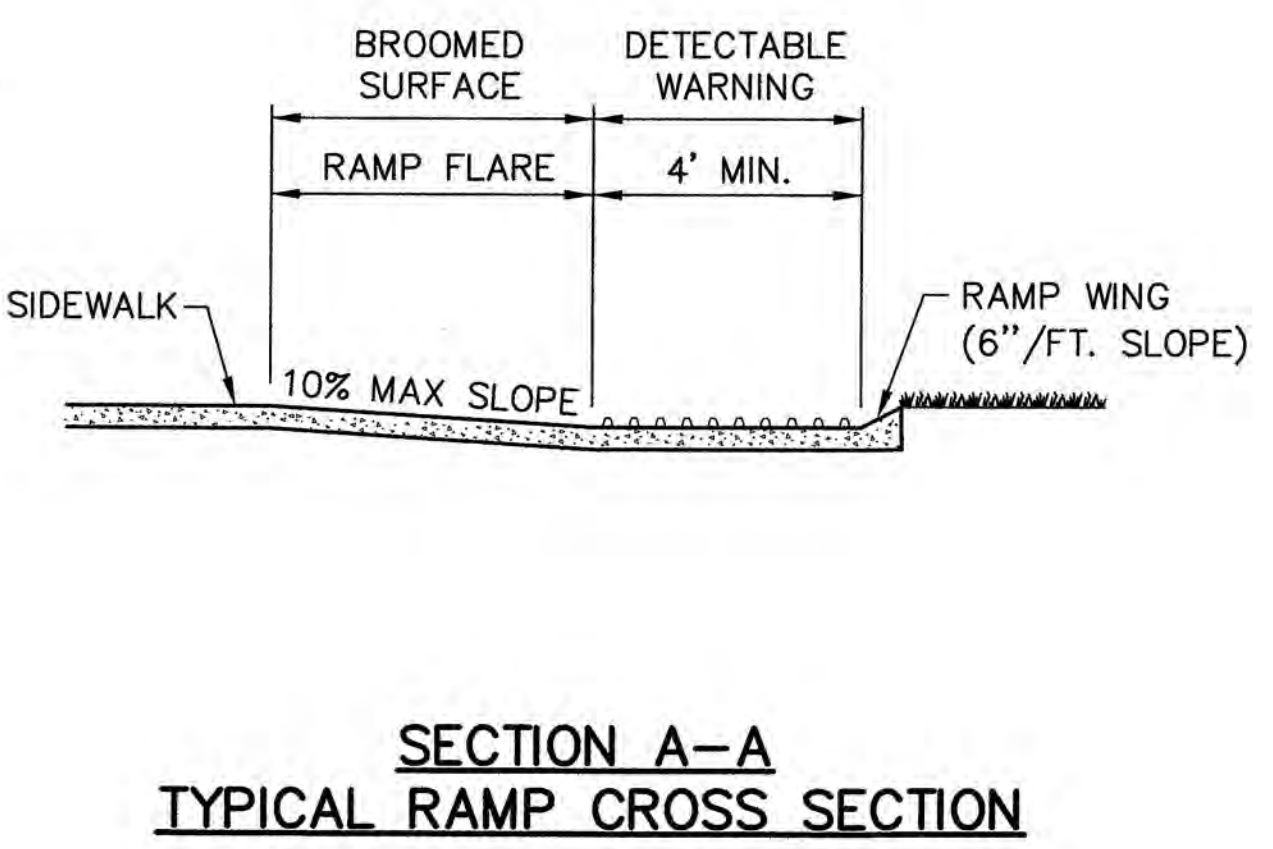
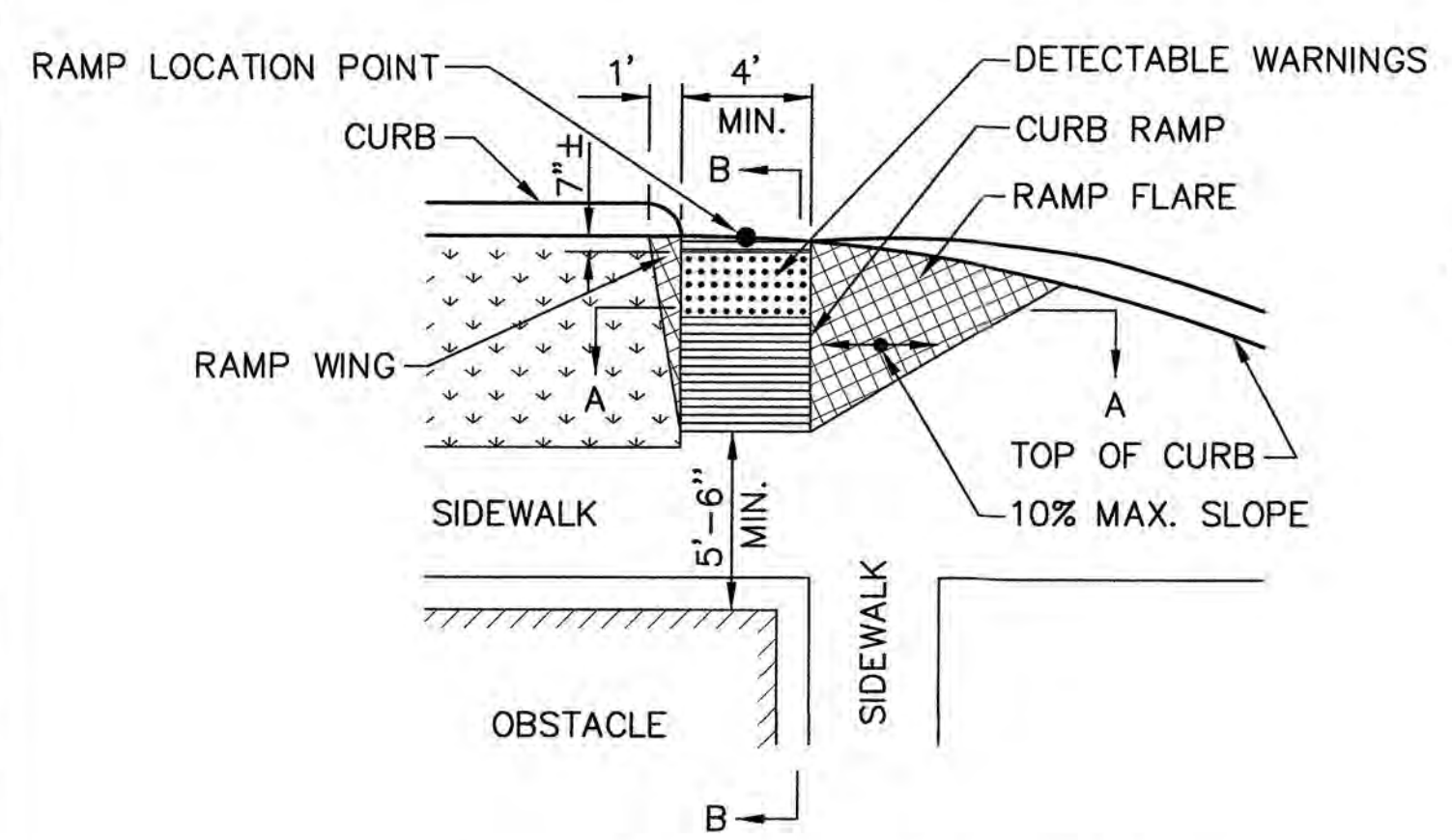
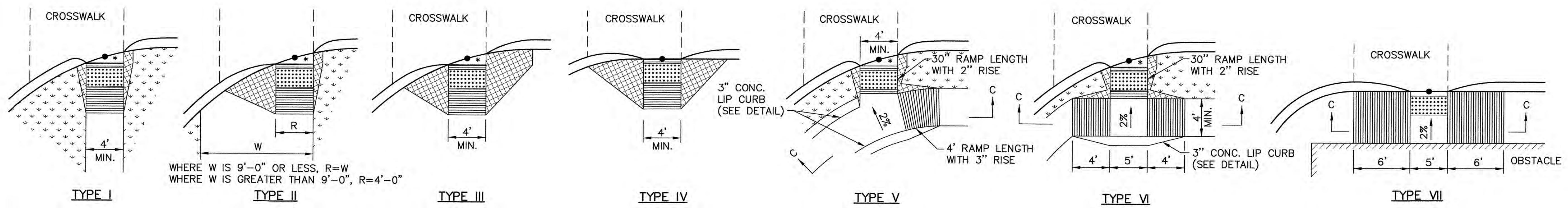
COMMERCE ROAD & FRONTAGE ROAD PAVING  
**PAVING DETAILS**  
LEXINGTON, NEBRASKA



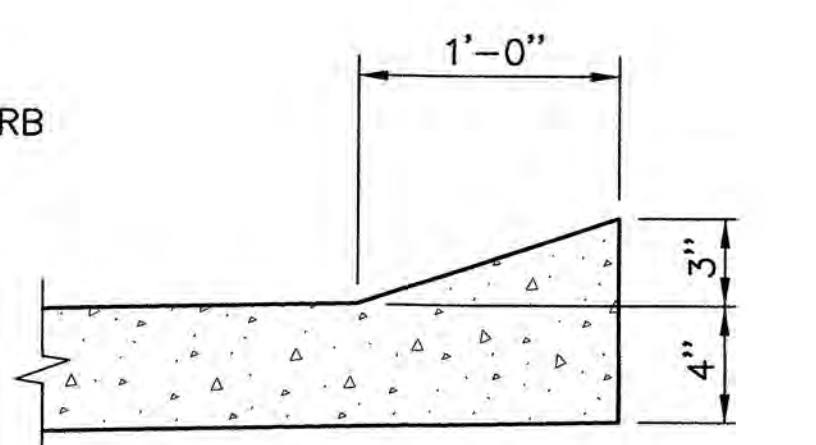
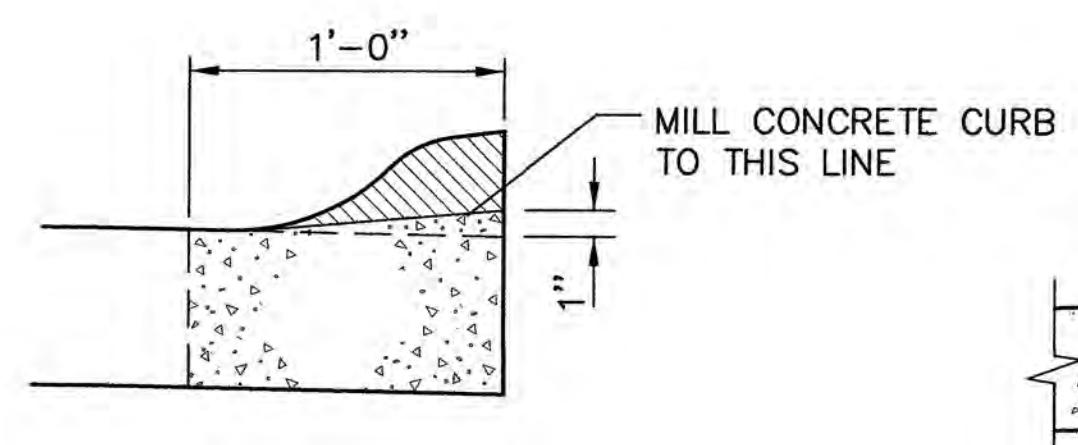
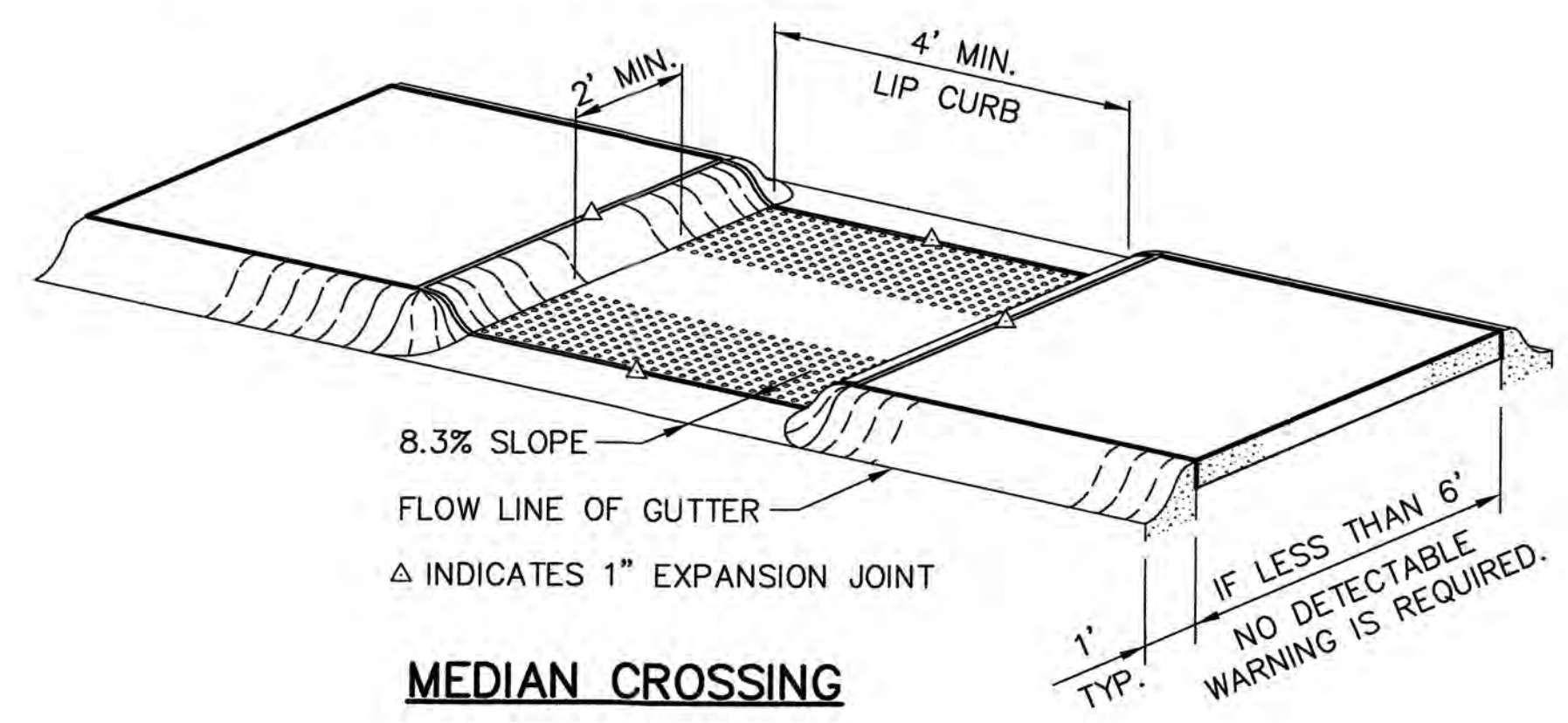
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SCALE:	
PROJECT NO.	127-A1-104
DATE:	AUGUST, 2018
FIELD BOOK	M&A DWG NO. 35309
DRAWN BY:	TLS
APRVD BY:	TRP
SHEET	10

G:\Projects\127\127-A1-104-05\Drawings\Sheet Drawings\SH\_GCN.dwg  
SAVED: 8/13/2018 9:48 AM  
PLOTED: 8/13/2018 9:48 AM



CURB RAMP	RISE	"L"	RAMP 1	RAMP 2
TYPE V	3"	VAR.	N/A	4'
TYPE VI	3"	5'	4'	4'
TYPE VII	5"	5'	6'	6'



NOTE: COMBINATION CONCRETE CURB AND GUTTER MAY BE REMOVED AND REPLACED IN LIEU OF MILLING.

NOTES:  
 THE NORMAL GUTTER LINE PROFILE SHALL BE MAINTAINED THROUGH THE AREA OF THE CURB RAMP.  
 THE SURFACE OF ALL CURB RAMPS SHALL BE TINED TRANSVERSELY TO THE SLOPE OF THE CURB RAMP. THE TINES SHALL PRODUCE GROOVES APPROXIMATELY 1/8" WIDE AND 3/16" DEEP ON 1/2" CENTERS. ALL FLARES AND WINGS SHALL BE BROOMED.  
 CARE SHALL BE TAKEN TO ASSURE A UNIFORM GRADE ON THE CURB RAMP, FREE OF SAGS AND SHORT GRADE CHANGES.

THE RAMP FLARES SHALL BE CONSTRUCTED WITH A 10% SLOPE AT RIGHT ANGLES TO THE SLOPE OF THE CURB RAMP, TYPES II, III & IV.

THE SLOPE OF SIDEWALKS APPROACHING CURB RAMPS (OR THEIR FLARES) SHALL BE FLAT ENOUGH TO PROVIDE RECOVERY AREAS FOR WHEELCHAIRS ENTERING OR EXITING THE RAMPS.

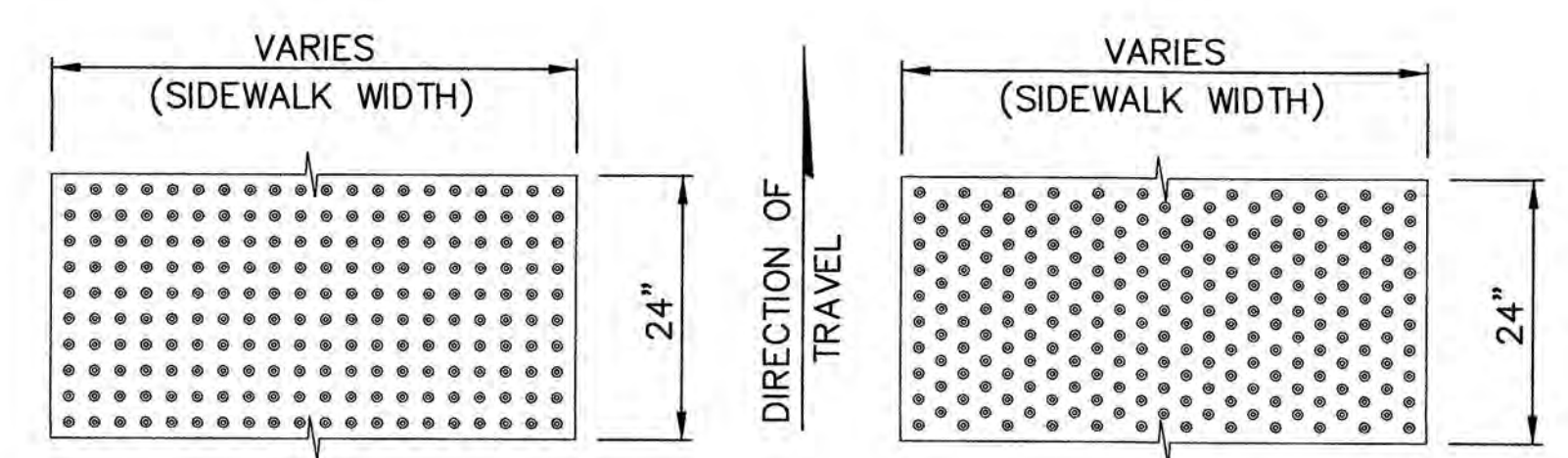
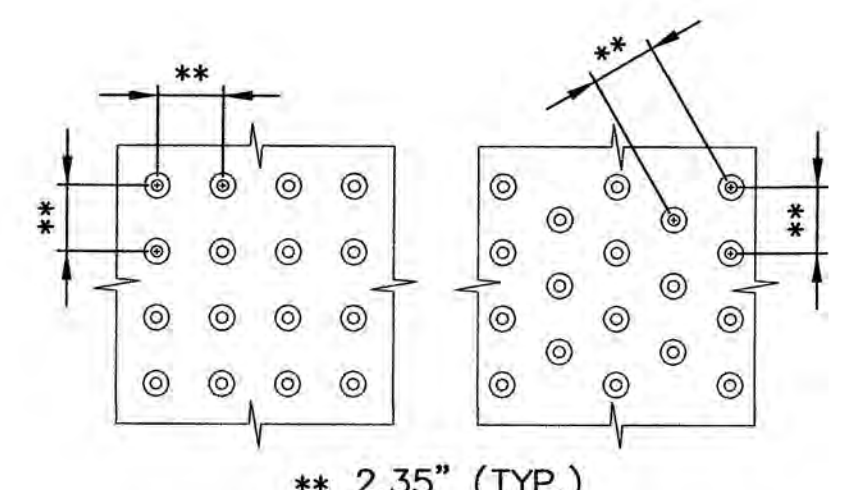
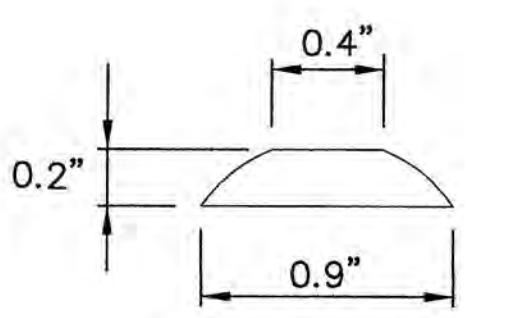
THE WORK OF CONSTRUCTING THESE RAMPS SHALL BE MEASURED AND PAID FOR AS A PART OF THE WORK FOR "CONCRETE SIDEWALKS", "CONCRETE MEDIAN SURFACING" OR "CONCRETE BIKEWAY". THE WORK OF MODIFICATION OF NEW OR EXISTING CURB WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED SUBSIDIARY TO OTHER ITEMS OF WORK FOR WHICH DIRECT PAYMENT IS MADE.

MAXIMUM PERCENT OF SLOPES ARE RELATIVE TO THE SLOPE OF THE ADJACENT SIDEWALK.

DETECTABLE WARNINGS SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP. THEY SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6" TO 8" FROM THE CURB LINE.

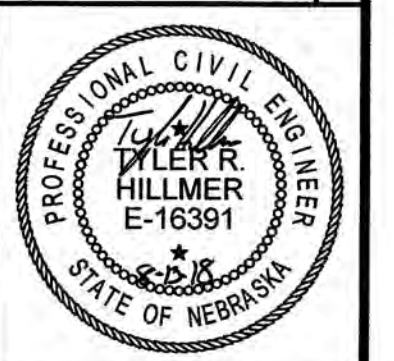
\* INDICATES 2% SLOPE TOWARDS STREET.

- LEGEND**
- DETECTABLE WARNINGS
  - TINED CURB RAMPS
  - BROOMED RAMP WINGS & FLARES
  - MILLED CONCRETE
  - GRASS OR NON WALKING SURFACE



**DETECTABLE WARNING DETAILS**

COMMERCE ROAD & FRONTAGE ROAD PAVING  
**HANDICAP RAMP DETAILS**  
 LEXINGTON, NEBRASKA



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SCALE:

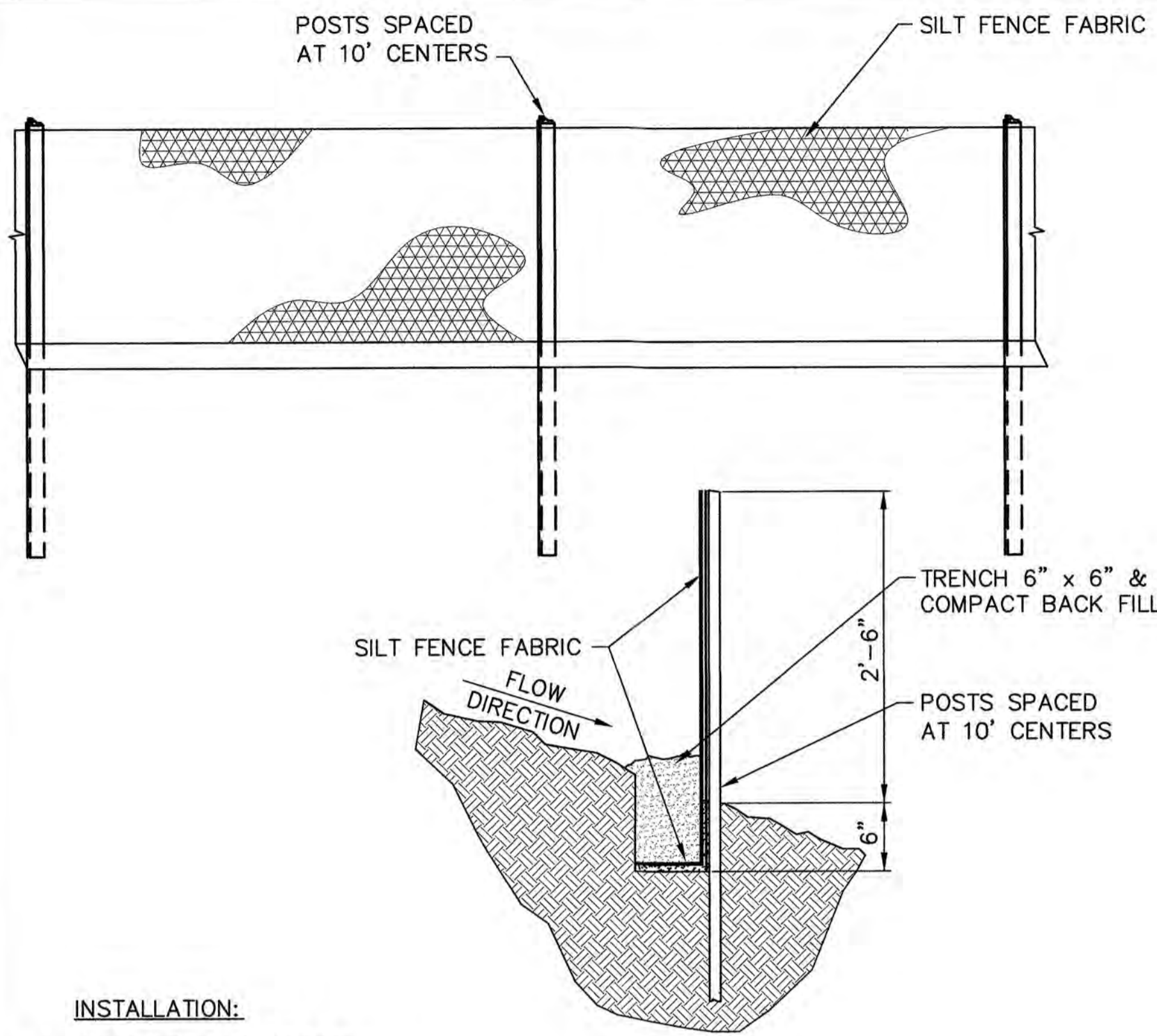
PROJECT NO. 127-A1-104

DATE: AUGUST, 2018

FIELD BOOK M&A DWG NO. 35308

DRAWN BY: TLS APRVD BY: 7/24

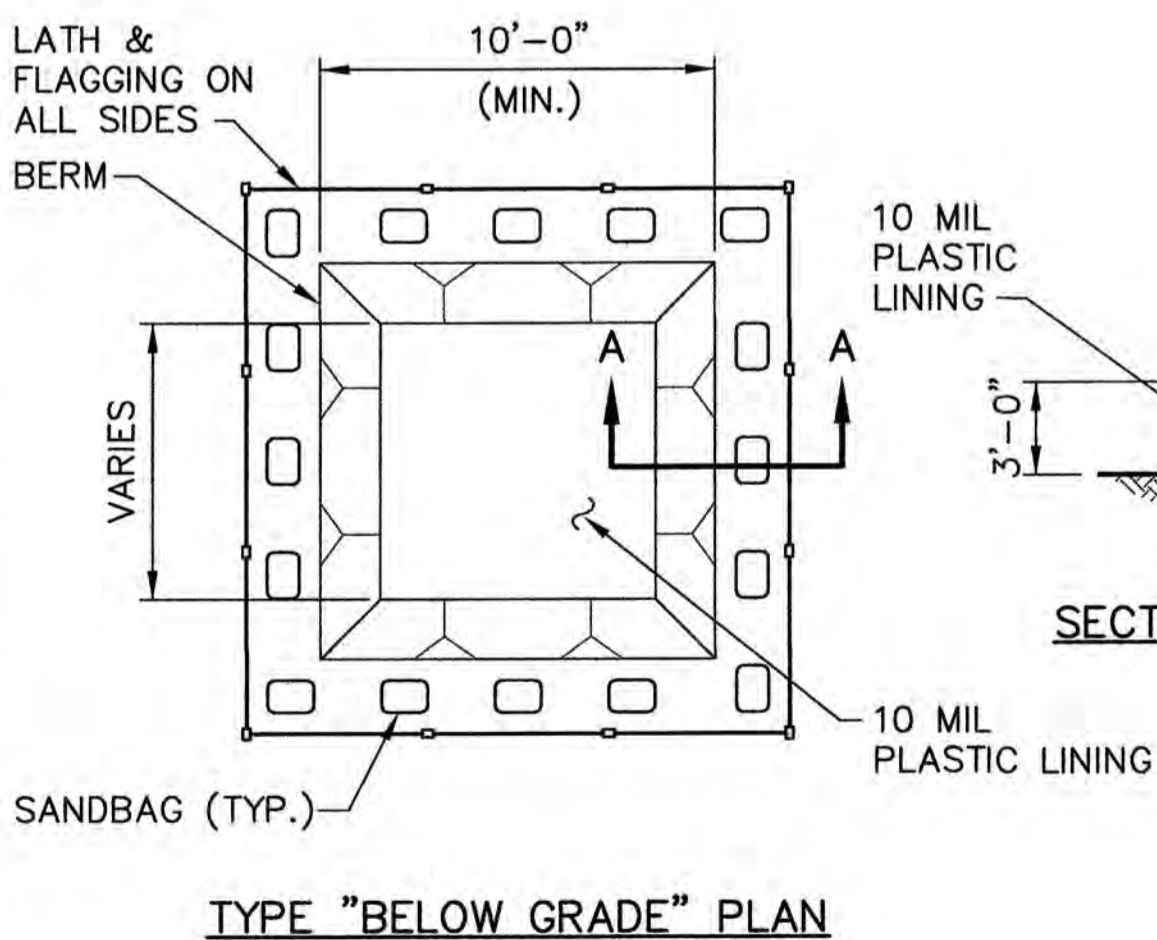
SHEET 11



**INSTALLATION:**

1. DIG A 6" X 6" TRENCH.
2. PLACE POSTS INTO GROUND WITH FABRIC ON UPSTREAM SIDE OF POST, PLACE FABRIC IN TRENCH.
3. USE EXCAVATED SOIL TO BACK FILL TRENCH.
4. WHEN CONNECTING TWO SECTIONS OF FABRIC OVERLAP A MINIMUM OF 6' AND USE 2 COMMON POSTS ON SPICE.
5. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL SHALL BE REMOVED WHEN 1/3 OF THE EXPOSED FABRIC IS COVERED.

**STANDARD SILT FENCE**  
NO SCALE

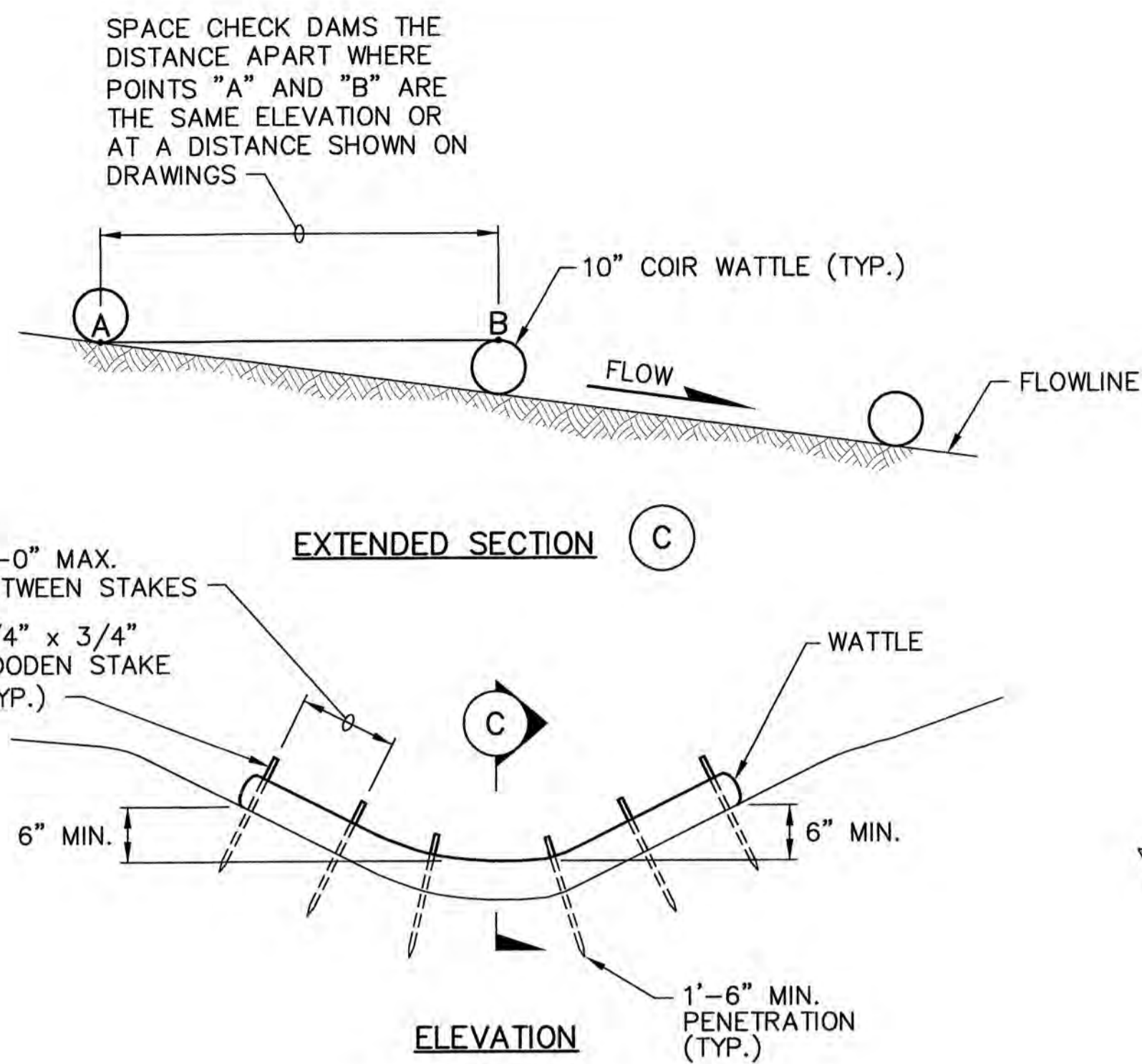


**TYPE "BELOW GRADE" PLAN**

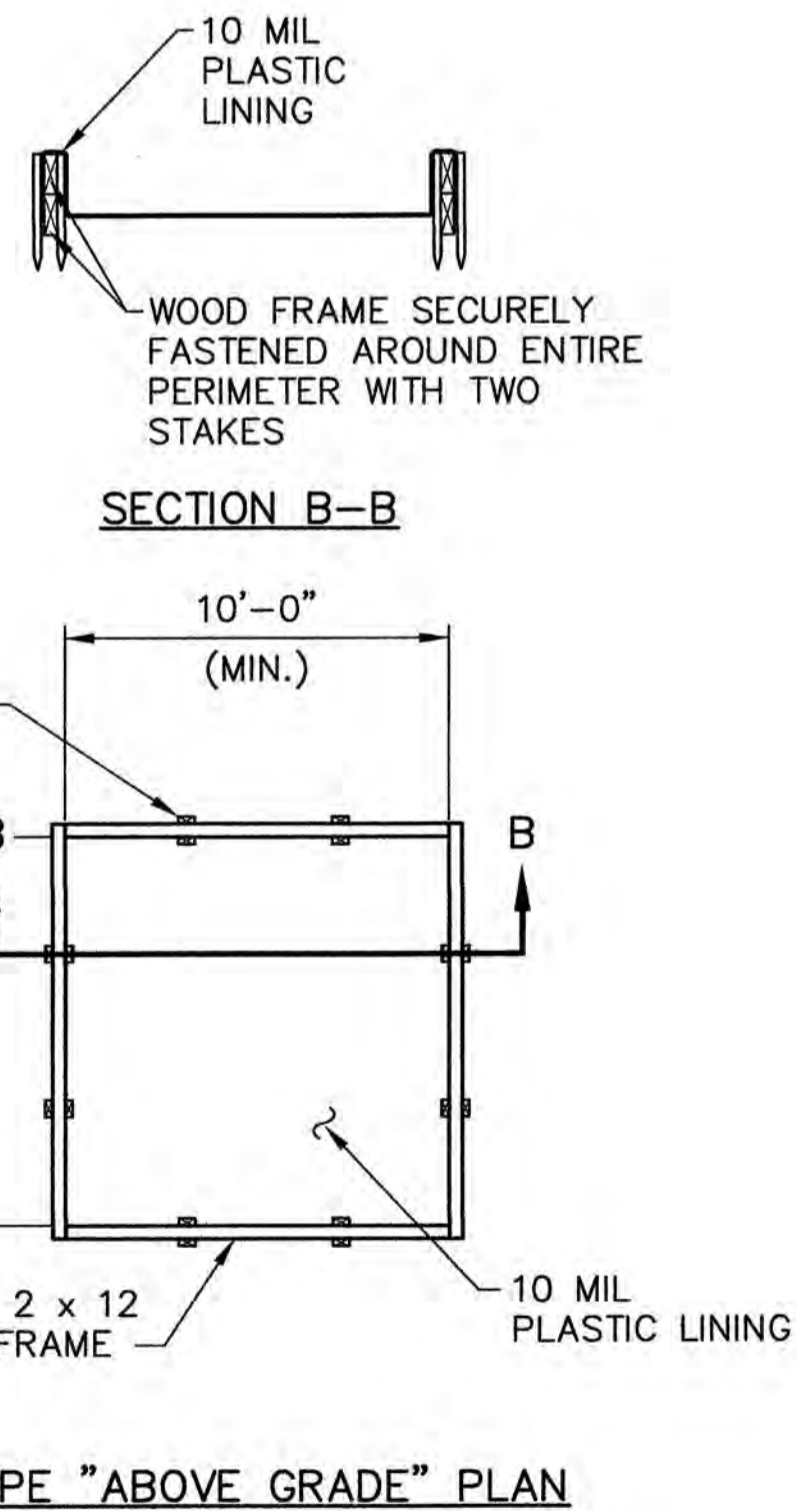
**NOTES:**

1. ACTUAL LAYOUT DETERMINED IN FIELD.
2. THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 30 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY.
3. CONTRACTOR TO DETERMINE ACTUAL SIZE REQUIRED TO MEET THE NEEDS FOR THE VOLUME OF CONCRETE TRUCK WASHOUT WATER ANTICIPATED PLUS RAINFALL.

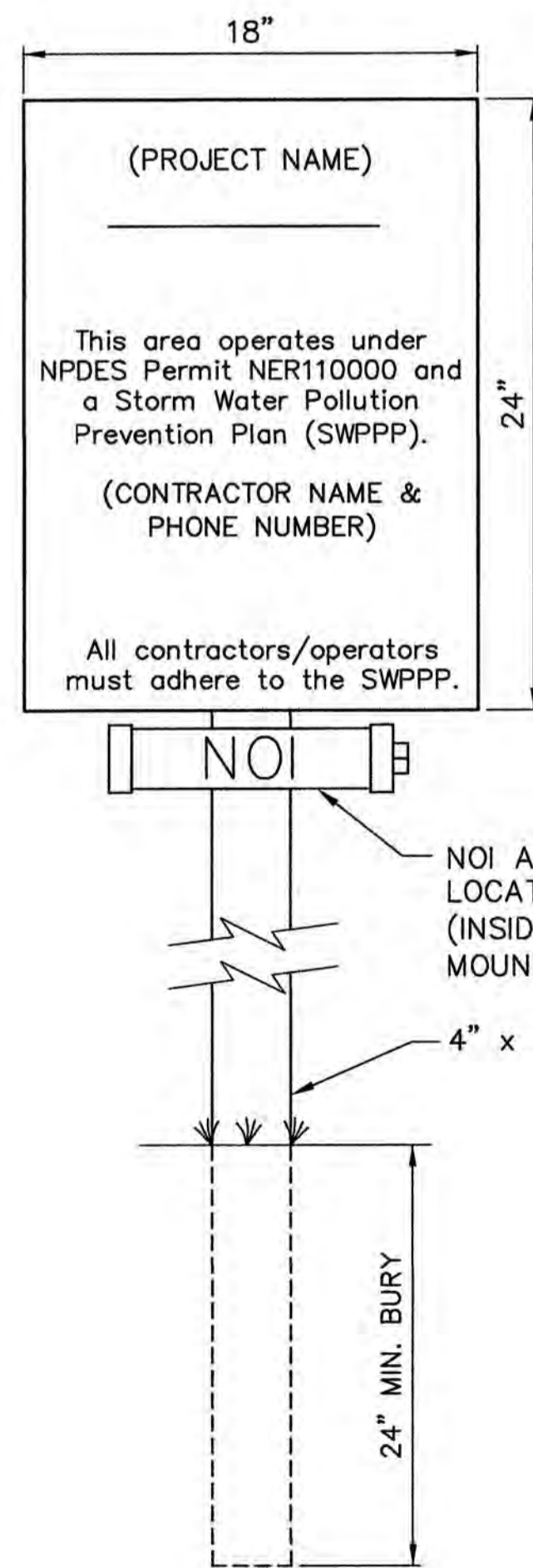
**CONCRETE WASHOUT PIT**  
NO SCALE



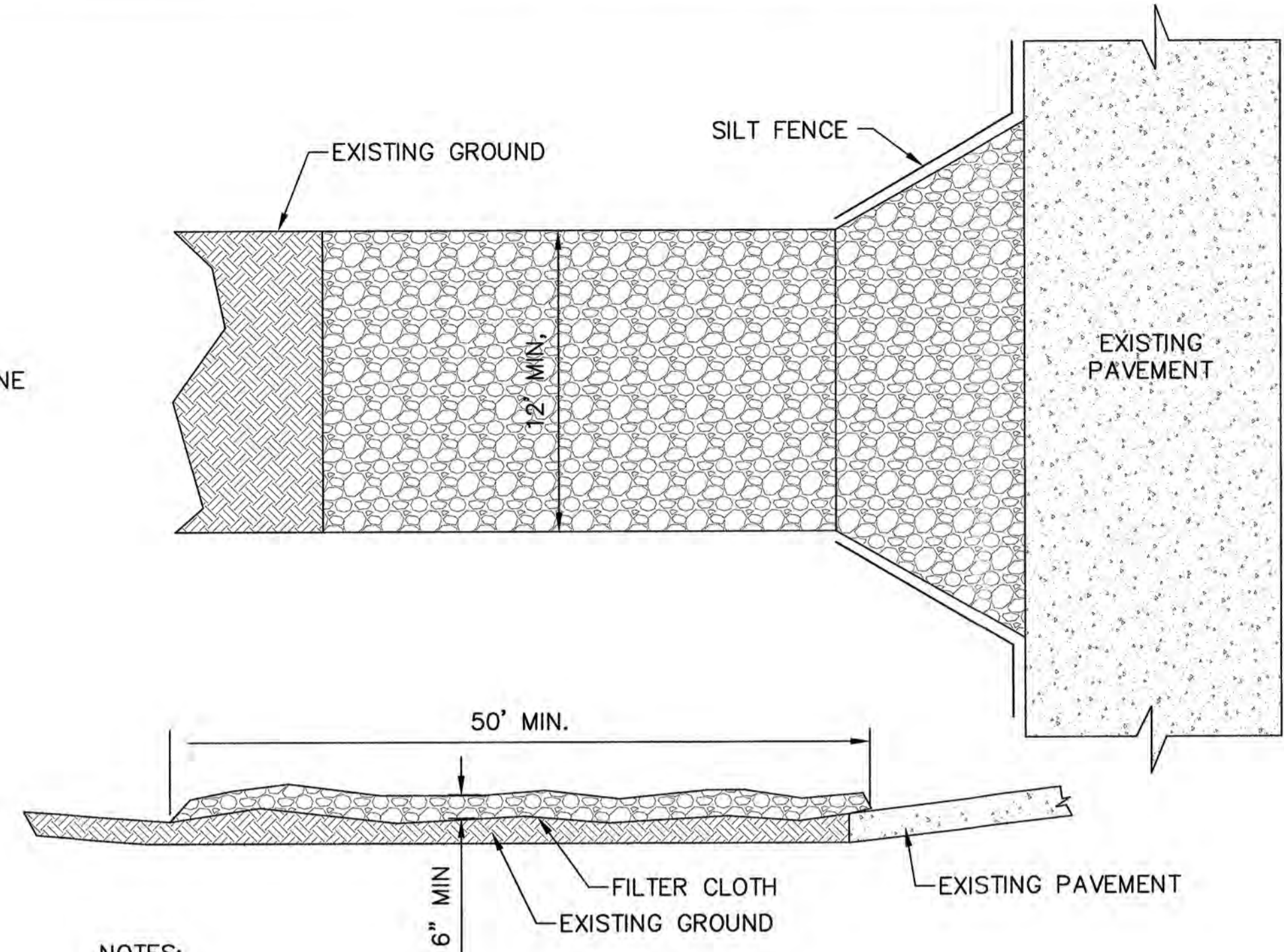
**COIR WATTLE SILT CHECK DETAIL**  
NO SCALE



**TYPE "ABOVE GRADE" PLAN**



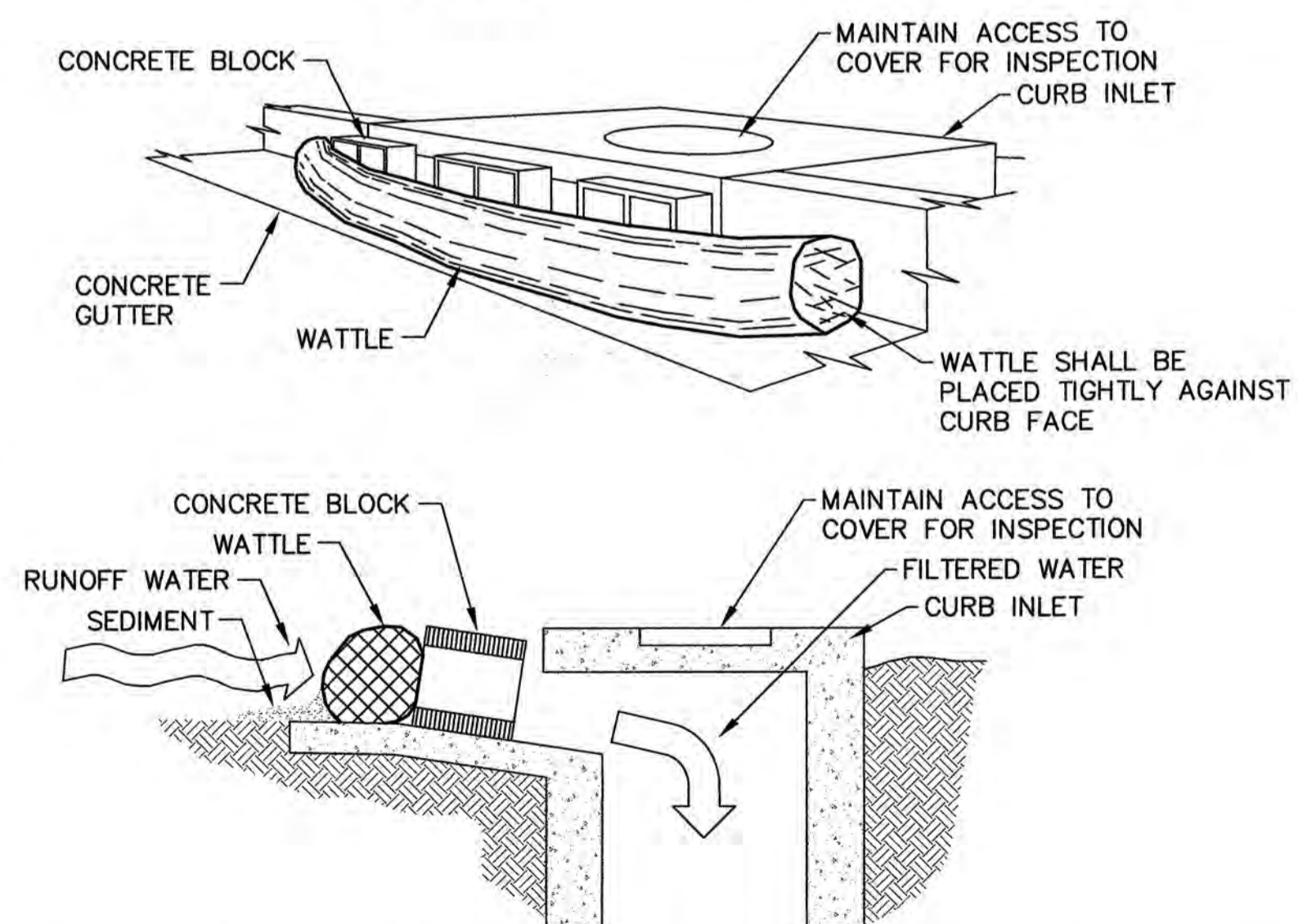
**SWPPP & PERMIT SIGN**  
NO SCALE



**NOTES:**

1. STONE SIZE - USE 2" STONE OR RECYCLED CONCRETE EQUIVALENT.
2. LENGTH - AS REQUIRED, BUT NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY).
3. THICKNESS - NOT LESS THAN SIX (6) INCHES.
4. WIDTH - TWELVE (12) FEET MINIMUM, BUT NOT LESS THAN THE FULL WIDTH OF POINTS WHERE INGRESS OR EGRESS OCCURS.
5. FILTER CLOTH - WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. FILTER WILL NOT BE REQUIRED ON A SINGLE FAMILY RESIDENCE LOT.
6. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION U TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
7. MAINTENANCE - THE ENTRANCE SHOULD BE MONITORED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
8. WASHING - WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS TO AN APPROVED SEDIMENT TRAPPING DEVICE.
9. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

**ROCK ENTRANCE ROAD DETAIL**  
NO SCALE



**NOTES:**

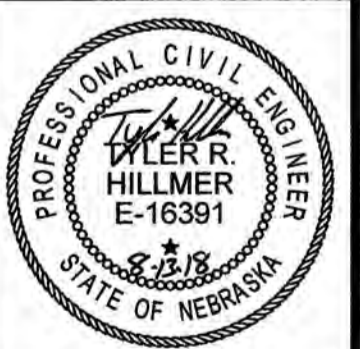
1. THIS METHOD OF INLET PROTECTION IS APPLICABLE AT CURB INLETS WHERE PONDING IN FRONT OF THE STRUCTURE IS NOT LIKELY TO CAUSE INCONVENIENCE OR DAMAGE TO ADJACENT STRUCTURES AND UNPROTECTED AREAS.
2. DEPENDING ON THE OPENING OF THE INLET, THE CONCRETE BLOCK MAY HAVE TO BE PLACED VERTICAL.

**CURB INLET PROTECTION**  
NO SCALE

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COMMERCE ROAD & FRONTAGE ROAD PAVING  
**EROSION CONTROL DETAILS**  
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SCALE:	PROJECT NO.
	127-A1-104
DATE:	AUGUST, 2018
FIELD BOOK	M&A DWG NO. 35307
DRAWN BY:	APR'D BY:
TLS	TRH
SHEET	

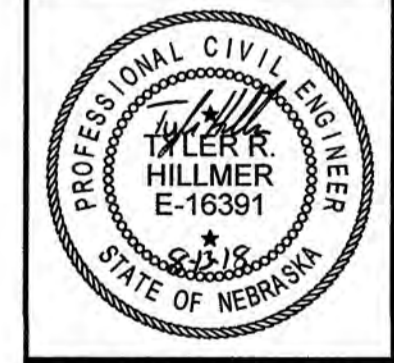


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COMMERCE ROAD & FRONTAGE ROAD PAVING  
**COMMERCE ROADWAY CROSS SECTIONS**  
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PROJECT NO. 127-A1-104  
 DATE: AUGUST, 2018  
 FIELD BOOK M&A DWG NO. 35306  
 DRAWN BY: TJS APRVD BY: TRJ  
 SHEET **X1**



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COMMERCE ROAD & FRONTAGE ROAD PAVING

**COMMERCE ROADWAY CROSS SECTIONS**

LEXINGTON, NEBRASKA

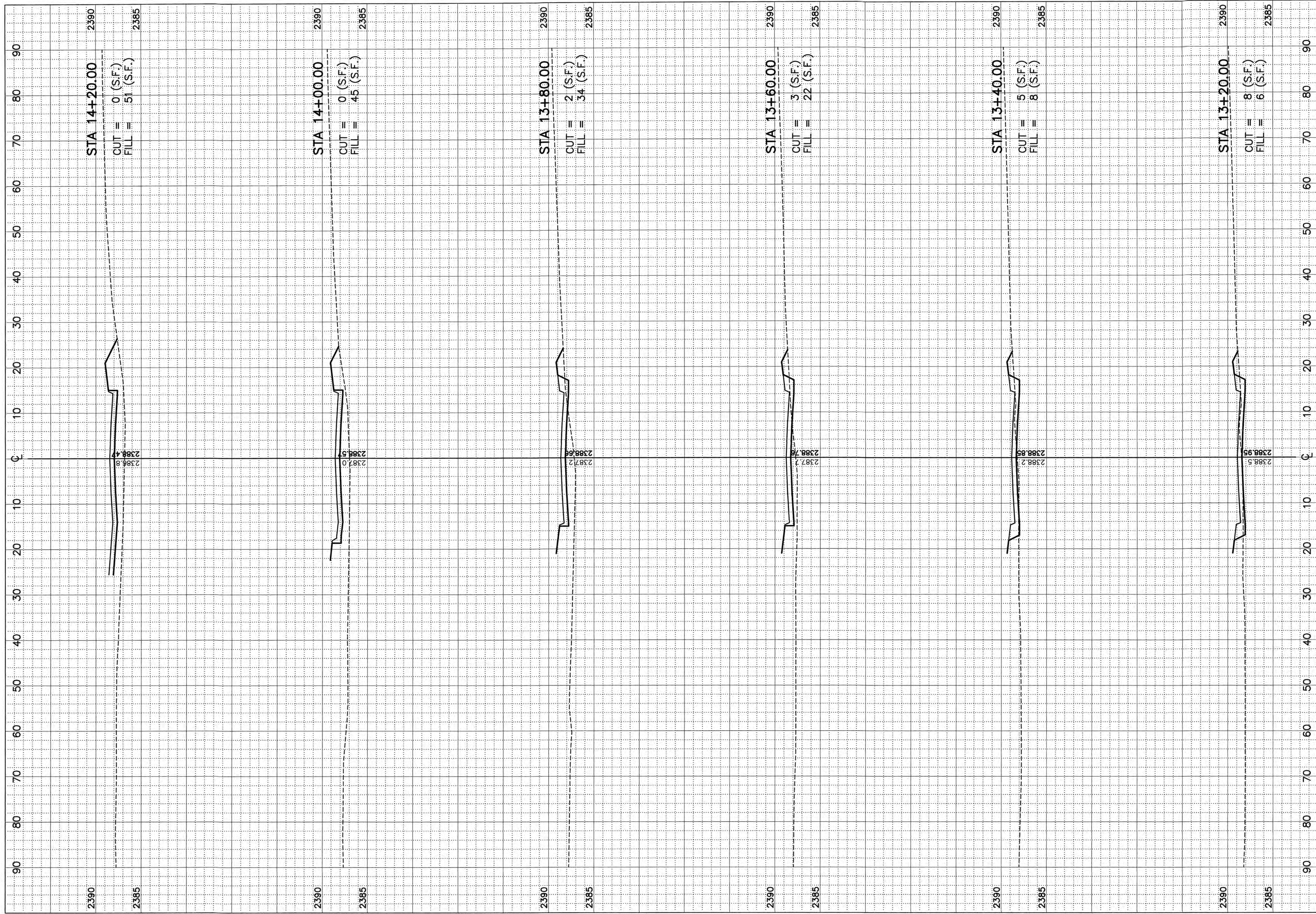


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 DATE: AUGUST, 2018  
 FIELD BOOK M&A DWG NO. 35305  
 DRAWN BY: TLS APRVD BY: TRL  
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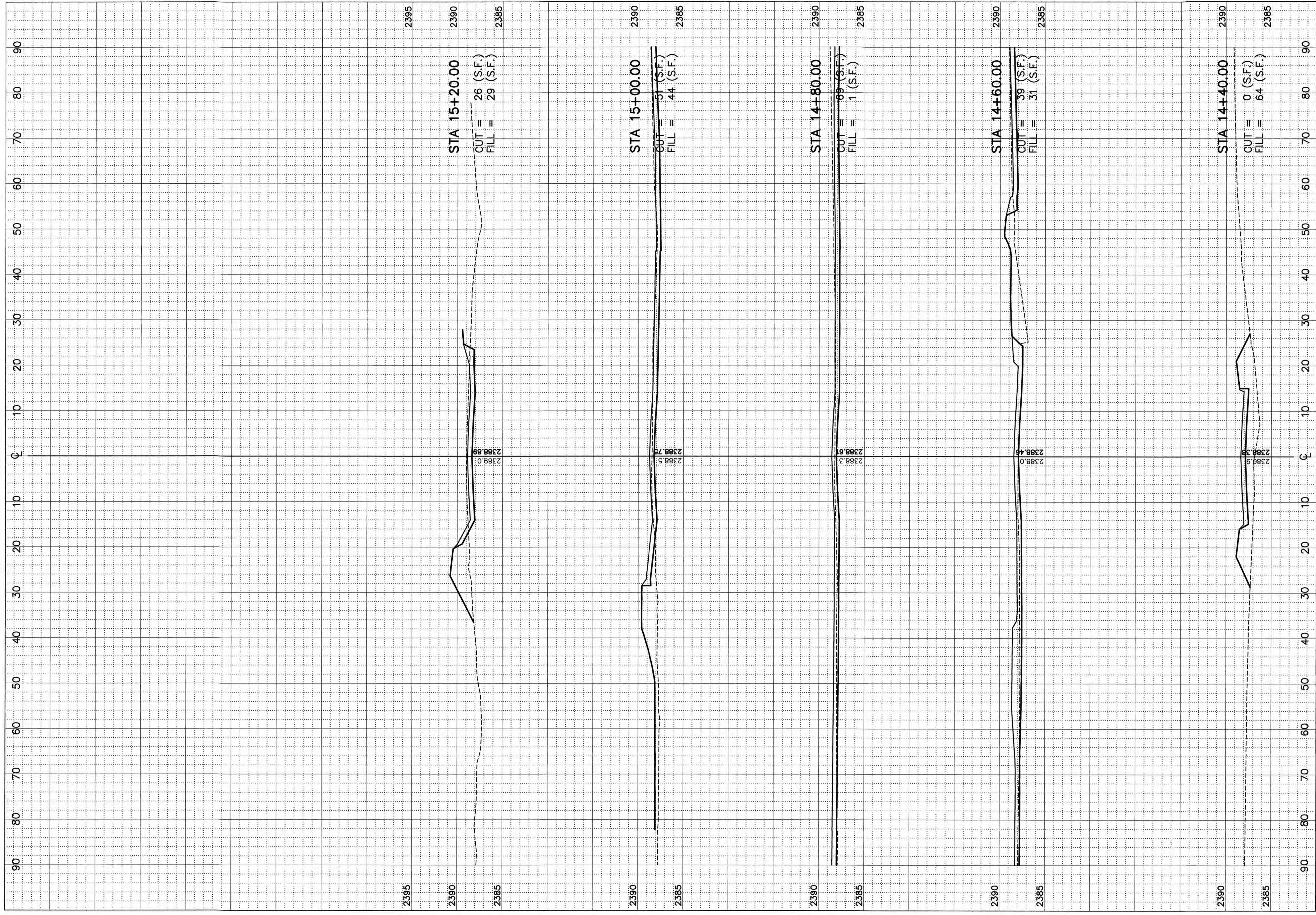


COMMERCE ROAD & FRONTAGE ROAD PAVING  
**COMMERCE ROADWAY CROSS SECTIONS**  
 LEXINGTON, NEBRASKA



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 DATE: AUGUST, 2018  
 FIELD BOOK M&A DWG NO. 35304  
 DRAWN BY: APRVD BY: TJS/TZM  
 SHEET **X3**

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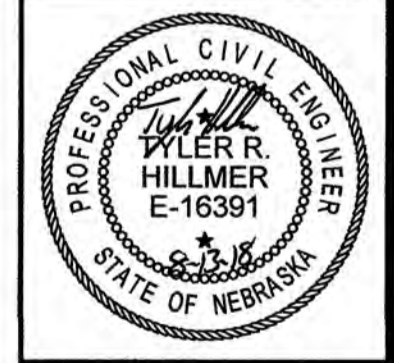


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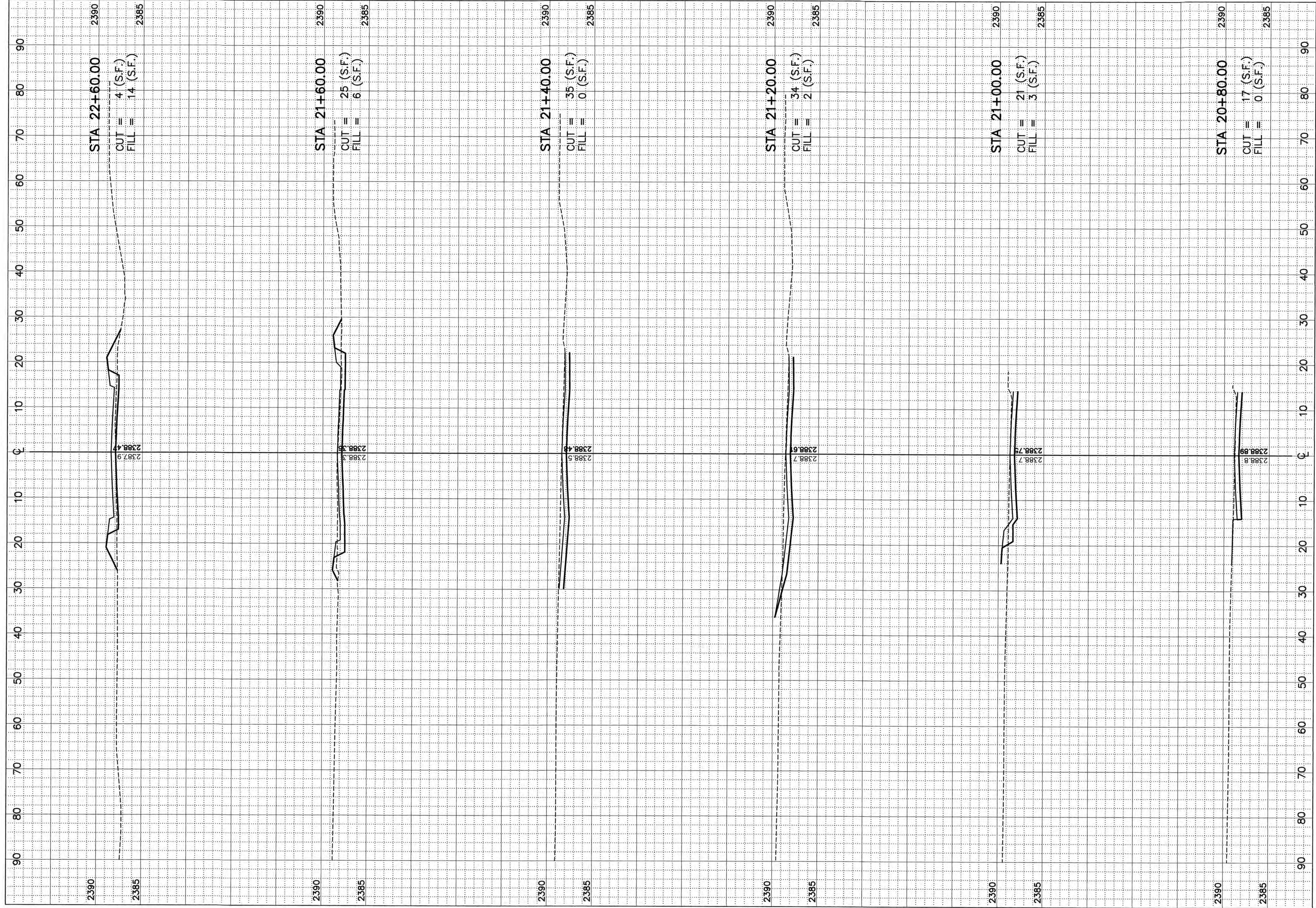
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PROJECT NO. 127-A1-104  
 DATE: AUGUST, 2018  
 FIELD BOOK M&A DWG NO. 35303  
 DRAWN BY: TILS  
 APPR'D BY: TRH

SHEET **X4**



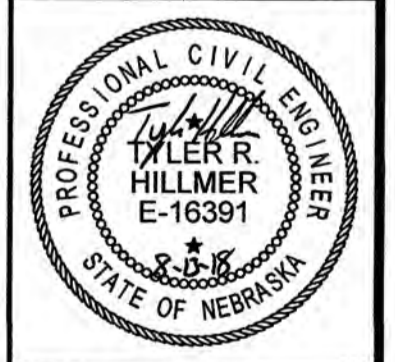


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COMMERCE ROAD & FRONTAGE ROAD PAVING  
**FRONTAGE ROADWAY CROSS SECTIONS**  
 LEXINGTON, NEBRASKA



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SCALE: \_\_\_\_\_

PROJECT NO. 127-A1-104

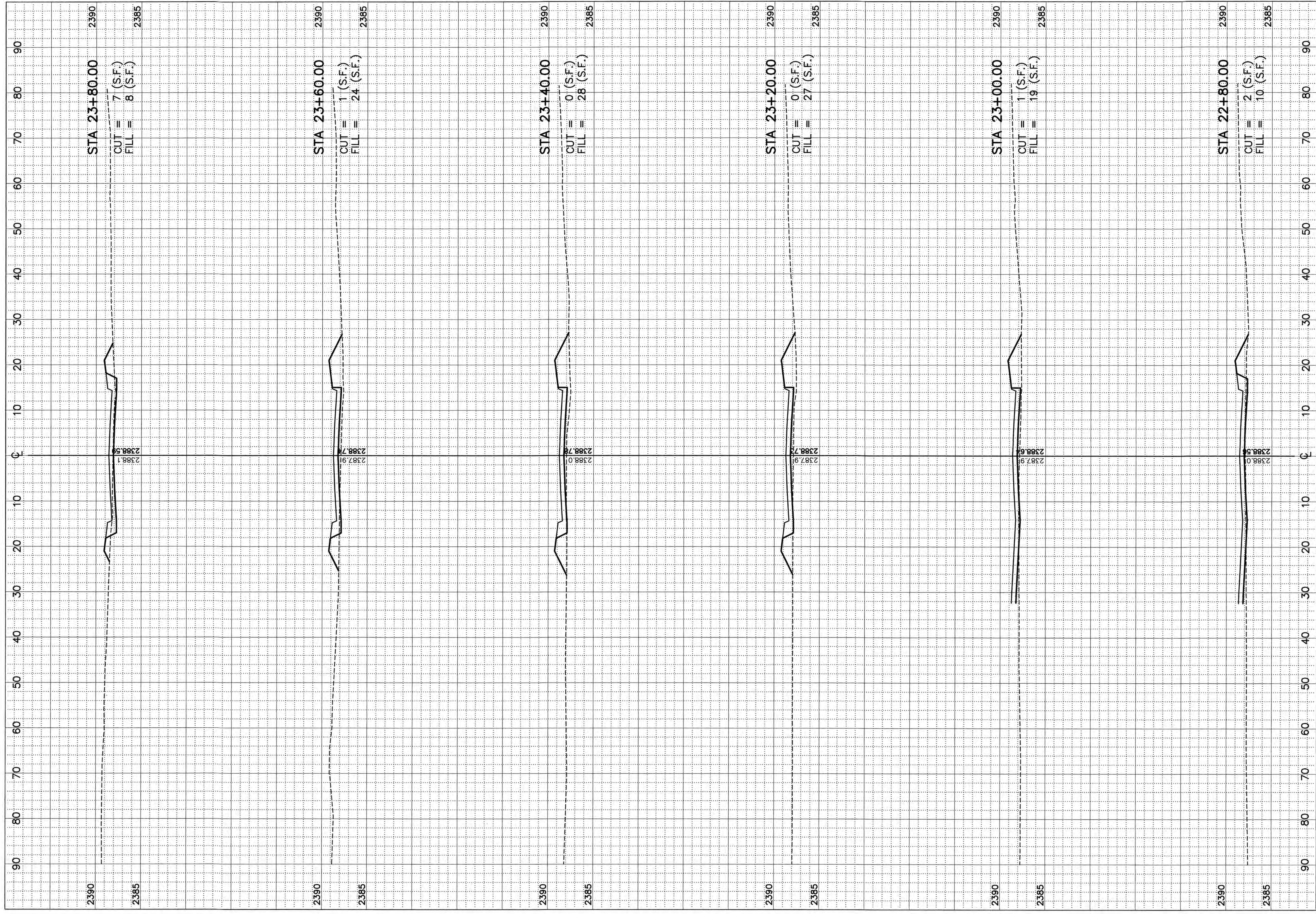
DATE: AUGUST, 2018

FIELD BOOK M&A DWG NO. 35302

DRAWN BY: APRVD BY: TRL

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**FRONTAGE ROADWAY CROSS SECTIONS**  
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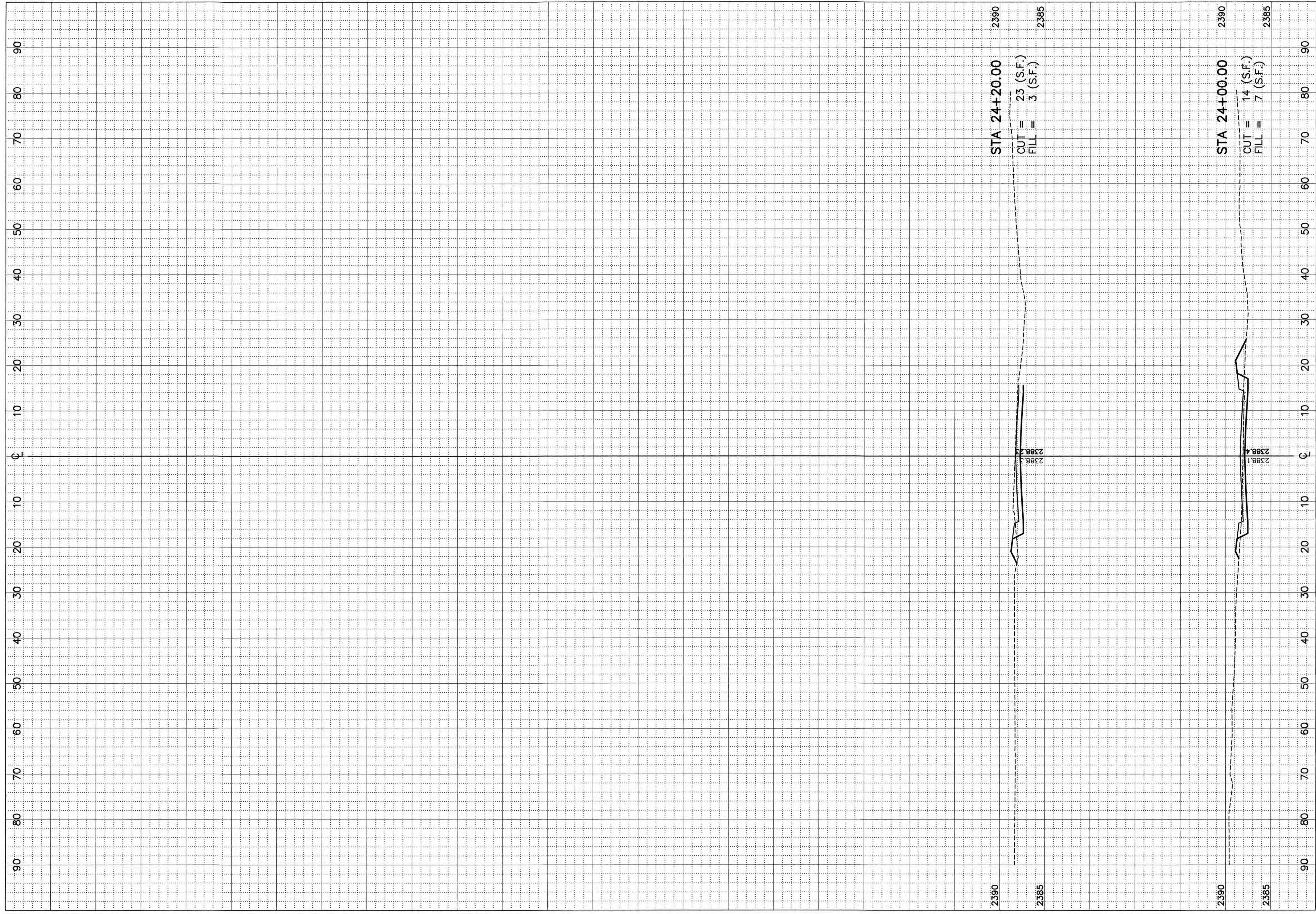
**PROFESSIONAL CIVIL ENGINEER**  
 HILLMER  
 E-16391  
 STATE OF NEBRASKA

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 DATE: AUGUST, 2018  
 FIELD BOOK M&A DWG NO. 35301  
 DRAWN BY: TJS      APR'D BY: TRH  
 SHEET \_\_\_\_\_

**X6**

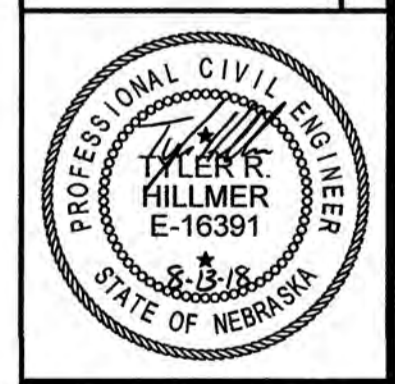


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COMMERCE ROAD & FRONTAGE ROAD PAVING  
**FRONTAGE ROADWAY CROSS SECTIONS**  
 LEXINGTON, NEBRASKA

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PROJECT NO. 127-A1-104  
 DATE: AUGUST, 2018  
 FIELD BOOK M&A DWG NO. 35300  
 DRAWN BY: TILS      APP'D BY: TRH  
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